

City of Scottsbluff, Nebraska

Tuesday, February 17, 2015

Regular Meeting

Item Consent3

Council to approve specifications for 27th Street and 2nd Avenue turn lane construction and authorize the city clerk to advertise for bids to be received by March 10, 2015, 2:00 p.m.

Staff Contact: Mark Bohl, Public Works Director

Advertisement for Bids
27th Street & 2nd Avenue Turn Lane Construction Project

Owner: City of Scottsbluff
Address: 2525 Circle Drive, Scottsbluff, NE 69361

Sealed Bids for the 27th Street & 2nd Avenue Turn Lane Construction Project for the City of Scottsbluff, will be received by Cindy Dickinson, City Clerk at City Hall, 2525 Circle Drive, Scottsbluff, Nebraska until 2:00 P.M., (Local Time) March 10, 2015, and then at said office publicly opened and read aloud.

The Contract Documents may be examined at the following locations:
City of Scottsbluff
2525 Circle Drive
Scottsbluff, NE 69361

M.C. Schaff & Associates
818 South Beltline Highway East
Scottsbluff, NE 69361

Copies of the Contract Documents may be obtained at the office of M.C. Schaff & Associates located at 818 South Beltline Highway East, Scottsbluff, NE 69361 upon payment of \$25.00 for each set, none of which will be refunded.

/s/ Cindy Dickinson
City Clerk

Publish three times:
2/20/2015, 2/27/2015, 3/6/2015

BID FORM FOR CONSTRUCTION CONTRACTS

Prepared by

ENGINEERS JOINT CONTRACT DOCUMENTS COMMITTEE

and

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ASSOCIATED GENERAL CONTRACTORS OF AMERICA

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A Practice Division of the
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Endorsed by



CONSTRUCTION SPECIFICATIONS INSTITUTE

EJCDC C-410 Bid Form for Construction Contracts
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www.acec.org

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(800) 548-2723
www.asce.org

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BID FORM

27th Street & 2nd Avenue Turn Lane Construction Project
Scottsbluff, Nebraska

TABLE OF CONTENTS

	Page
Article 1 – Bid Recipient.....	1
Article 2 – Bidder’s Acknowledgements.....	1
Article 3 – Bidder’s Representations	1
Article 4 – Bidder’s Certification.....	2
Article 5 – Basis of Bid	3
Article 6 – Time of Completion	4
Article 7 – Attachments to this Bid.....	5
Article 8 – Defined Terms.....	5
Article 9 – Bid Submittal.....	5

ARTICLE 1 – BID RECIPIENT

1.01 This Bid is submitted to:

City of Scottsbluff
2525 Circle Drive
Scottsbluff, NE 69361

1.02 The undersigned Bidder proposes and agrees, if this Bid is accepted, to enter into an Agreement with Owner in the form included in the Bidding Documents to perform all Work as specified or indicated in the Bidding Documents for the prices and within the times indicated in this Bid and in accordance with the other terms and conditions of the Bidding Documents.

ARTICLE 2 – BIDDER’S ACKNOWLEDGEMENTS

2.01 Bidder accepts all of the terms and conditions of the Instructions to Bidders, including without limitation those dealing with the disposition of Bid security. This Bid will remain subject to acceptance for 60 days after the Bid opening, or for such longer period of time that Bidder may agree to in writing upon request of Owner.

ARTICLE 3 – BIDDER’S REPRESENTATIONS

3.01 In submitting this Bid, Bidder represents that:

A. Bidder has examined and carefully studied the Bidding Documents, other related data identified in the Bidding Documents, and the following Addenda, receipt of which is hereby acknowledged:

<u>Addendum No.</u>	<u>Addendum Date</u>
_____	_____
_____	_____
_____	_____

B. Bidder has visited the Site and become familiar with and is satisfied as to the general, local, and Site conditions that may affect cost, progress, and performance of the Work.

C. Bidder is familiar with and is satisfied as to all Laws and Regulations that may affect cost, progress, and performance of the Work.

D. Bidder has carefully studied all: (1) reports of explorations and tests of subsurface conditions at or contiguous to the Site and all drawings of physical conditions relating to existing surface or subsurface structures at the Site (except Underground Facilities) that have been identified in SC-4.02 as containing reliable "technical data," and (2) reports and drawings of Hazardous Environmental Conditions, if any, at the Site that have been identified in SC-4.06 as containing reliable "technical data."

E. Bidder has considered the information known to Bidder; information commonly known to contractors doing business in the locality of the Site; information and observations obtained

from visits to the Site; the Bidding Documents; and the Site-related reports and drawings identified in the Bidding Documents, with respect to the effect of such information, observations, and documents on (1) the cost, progress, and performance of the Work; (2) the means, methods, techniques, sequences, and procedures of construction to be employed by Bidder, including applying the specific means, methods, techniques, sequences, and procedures of construction expressly required by the Bidding Documents; and (3) Bidder's safety precautions and programs.

- F. Based on the information and observations referred to in Paragraph 3.01.E above, Bidder does not consider that further examinations, investigations, explorations, tests, studies, or data are necessary for the determination of this Bid for performance of the Work at the price(s) bid and within the times required, and in accordance with the other terms and conditions of the Bidding Documents.
 - G. Bidder is aware of the general nature of work to be performed by Owner and others at the Site that relates to the Work as indicated in the Bidding Documents.
 - H. Bidder has given Engineer written notice of all conflicts, errors, ambiguities, or discrepancies that Bidder has discovered in the Bidding Documents, and the written resolution thereof by Engineer is acceptable to Bidder.
1. The Bidding Documents are generally sufficient to indicate and convey understanding of all terms and conditions for the performance of the Work for which this Bid is submitted.

ARTICLE 4 – BIDDER'S CERTIFICATION

4.01 Bidder certifies that:

- A. This Bid is genuine and not made in the interest of or on behalf of any undisclosed individual or entity and is not submitted in conformity with any collusive agreement or rules of any group, association, organization, or corporation;
- B. Bidder has not directly or indirectly induced or solicited any other Bidder to submit a false or sham Bid;
- C. Bidder has not solicited or induced any individual or entity to refrain from bidding; and
- D. Bidder has not engaged in corrupt, fraudulent, collusive, or coercive practices in competing for the Contract. For the purposes of this Paragraph 4.01.D:
 1. "corrupt practice" means the offering, giving, receiving, or soliciting of anything of value likely to influence the action of a public official in the bidding process;
 2. "fraudulent practice" means an intentional misrepresentation of facts made (a) to influence the bidding process to the detriment of Owner, (b) to establish bid prices at artificial non-competitive levels, or (c) to deprive Owner of the benefits of free and open competition;

3. "collusive practice" means a scheme or arrangement between two or more Bidders, with or without the knowledge of Owner, a purpose of which is to establish bid prices at artificial, non-competitive levels; and
4. "coercive practice" means harming or threatening to harm, directly or indirectly, persons or their property to influence their participation in the bidding process or affect the execution of the Contract.

ARTICLE 5 – BASIS OF BID

Bidder will complete the Work in accordance with the Contract Documents for the following price(s):

27th Street & 2nd Avenue Turn Lane Construction Project

<u>Item</u>	<u>Description</u>	<u>Estimated Quantity</u>	<u>Unit</u>	<u>Unit Price</u>	<u>Total Estimated Price</u>
1	Mobilization	1	LS	\$ _____	\$ _____
2	Traffic Control	1	LS	\$ _____	\$ _____
3	Remove Existing Pavement, Curb, Gutter, Sidewalk, Etc.	3,900	SF	\$ _____	\$ _____
4	Remove Storm Drain Inlet	1	EA	\$ _____	\$ _____
5	Remove Storm Drain Pipe	10	LF	\$ _____	\$ _____
6	Remove Existing Pavement Markings	2,045	LF	\$ _____	\$ _____
7	8-Inch P.C. Concrete Pavement	250	SY	\$ _____	\$ _____
8	30-Inch P.C. Concrete Curb & Gutter	260	LF	\$ _____	\$ _____
9	30-Foot Intersection Radius	1	EA	\$ _____	\$ _____
10	6-Inch P.C. Concrete Driveway	325	SF	\$ _____	\$ _____
11	4-Inch P.C. Concrete Sidewalks	2,025	SF	\$ _____	\$ _____
12	Handicap Ramp	1	EA	\$ _____	\$ _____
13	Standard Storm Drain Inlet	2	EA	\$ _____	\$ _____
14	Storm Drain Junction Box	2	EA	\$ _____	\$ _____

15	12-Inch R.C.C.P. Storm Drain	85	LF	\$ _____	\$ _____
16	Relocate Existing Fire Hydrant and Valve	1	EA	\$ _____	\$ _____
17	Adjust Valve Box to Grade	1	EA	\$ _____	\$ _____
18	Adjust Pull Box to Grade	1	EA	\$ _____	\$ _____
19	Adjust Manhole to Grade	1	EA	\$ _____	\$ _____
20	4-Inch White Wet Reflective Preformed Pavement Marking, Type 4, Grooved	210	LF	\$ _____	\$ _____
21	4-Inch Yellow Wet Reflective Preformed Pavement Marking, Type 4, Grooved	1,600	LF	\$ _____	\$ _____
22	6-Inch White Wet Reflective Preformed Pavement Marking, Type 4, Grooved	385	LF	\$ _____	\$ _____
23	12-Inch Yellow Wet Reflective Preformed Pavement Marking, Type 4, Grooved	210	LF	\$ _____	\$ _____
24	24-Inch White Preformed Pavement Marking, Type 4, Grooved	15	LF	\$ _____	\$ _____
25	White Left Turn Arrow Wet Reflective Preformed Pavement Marking, Type 4, Grooved	4	EA	\$ _____	\$ _____
26	Sodding	905	SF	\$ _____	\$ _____
TOTAL ITEMS 1-26				\$ _____	\$ _____

Unit Prices have been computed in accordance with Paragraph 11.03.B of the General Conditions.

Bidder acknowledges that this contract is a lump sum contract for each schedule. No additional payment will be made for extras without an approved change order.

ARTICLE 6 – TIME OF COMPLETION

6.01 Bidder agrees that the Work will be substantially complete and will be completed and ready for final payment in accordance with Paragraph 14.07 of the General Conditions on or before the dates or within the number of calendar days indicated in the Agreement.

6.02 Bidder accepts the provisions of the Agreement as to liquidated damages.

ARTICLE 7 – ATTACHMENTS TO THIS BID

The following documents are submitted with and made a condition of this bid:

- A. Required Bid security in the form of a Bid Bond (EJCDC No. C-430) or Certified Check (circle type of security provided);
- B. List of Project References;

ARTICLE 8 – DEFINED TERMS

8.01 The terms used in this Bid with initial capital letters have the meanings stated in the Instructions to Bidders, the General Conditions, and the Supplementary Conditions.

ARTICLE 9 – BID SUBMITTAL

9.01 This Bid is submitted by:

If Bidder is:

An Individual

Name (typed or printed): _____

By: _____
(Individual's signature)

Doing business as: _____

A Partnership

Partnership Name: _____

By: _____
(Signature of general partner -- attach evidence of authority to sign)

Name (typed or printed): _____

A Corporation

Corporation Name: _____ (SEAL)

State of Incorporation: _____

Type (General Business, Professional, Service, Limited Liability): _____

By: _____

(Signature -- attach evidence of authority to sign)

Name (typed or printed): _____

Title: _____

(CORPORATE SEAL)

Attest _____

Date of Qualification to do business in NEBRASKA is ____/____/____.

A Joint Venture

Name of Joint Venture: _____

First Joint Venturer Name: _____ (SEAL)

By: _____

(Signature of first joint venture partner -- attach evidence of authority to sign)

Name (typed or printed): _____

Title: _____

Second Joint Venturer Name: _____ (SEAL)

By: _____

(Signature of second joint venture partner -- attach evidence of authority to sign)

Name (typed or printed): _____

Title: _____

(Each joint venturer must sign. The manner of signing for each individual, partnership, and corporation that is a party to the joint venture should be in the manner indicated above.)

Bidder's Business Address _____

Phone No. _____ **Fax No.** _____

E-mail _____

SUBMITTED on _____, 20____.

State Contractor License No. _____ *[If applicable]*

Cost Estimate

27th Street & 2nd Avenue Turn Lane Construction Project						
No.	Description	Unit	Quantity	Unit Cost	Total	
1	Mobilization	LS	1	\$ 8,000.00	\$	8,000.00
2	Traffic Control	LS	1	\$ 5,000.00	\$	5,000.00
3	Remove Existing Pavement, Curb, Gutter, Etc	SF	3900	\$ 1.50	\$	5,850.00
4	Remove Existing Inlet	EA	1	\$ 250.00	\$	250.00
5	Remove Storm Drain Pipe	LF	10	\$ 20.00	\$	200.00
6	Remove Existing Pavement Marking	LF	2045	\$ 2.00	\$	4,090.00
7	8-Inch P.C. Concrete Pavement	SY	250	\$ 45.00	\$	11,250.00
8	30-Inch P.C. Concrete Curb & Gutter	LF	260	\$ 20.00	\$	5,200.00
9	30-Foot Intersection Radius	EA	1	\$ 3,500.00	\$	3,500.00
10	6-Inch P.C. Concrete Driveway	SF	325	\$ 6.00	\$	1,950.00
11	4-Inch P.C. Concrete Sidewalk	SF	2025	\$ 5.00	\$	10,125.00
12	Handicap Ramp	EA	1	\$ 1,200.00	\$	1,200.00
13	Standard Storm Drain Inlet	EA	2	\$ 2,000.00	\$	4,000.00
14	Storm Drain Junction Box	EA	2	\$ 2,000.00	\$	4,000.00
15	12-Inch RCCP Storm Drain Pipe	LF	85	\$ 40.00	\$	3,400.00
16	Relocate Fire Hydrant & Valve	EA	1	\$ 1,500.00	\$	1,500.00
17	Adjust Valve Box to Grade	EA	1	\$ 400.00	\$	400.00
18	Adjust Pull Box to Grade	EA	1	\$ 400.00	\$	400.00
19	Adjust Manhole to Grade	EA	1	\$ 500.00	\$	500.00
20	4-Inch White Wet Reflective Preformed Pavement Marking, Type 4, Grooved	EA	210	\$ 4.00	\$	840.00
21	4-Inch Yellow Wet Reflective Preformed Pavement Marking, Type 4, Grooved	LF	1600	\$ 4.00	\$	6,400.00
22	6-Inch White Wet Reflective Preformed Pavement Marking, Type 4, Grooved	LF	385	\$ 6.00	\$	2,310.00
23	12-Inch Yellow Wet Reflective Preformed Pavement Marking, Type 4, Grooved	LF	210	\$ 12.00	\$	2,520.00
24	24-Inch White Preformed Pavement Marking, Type 4, Grooved	LF	15	\$ 25.00	\$	375.00
25	White Left Turn Arrow Wet Reflective Preformed Pavement Marking, Type 4, Grooved	EA	4	\$ 500.00	\$	2,000.00
26	Sodding	SF	905.00	\$ 1.50	\$	1,357.50
Total Items 1-26					\$	86,617.50

CITY OF SCOTTSBLUFF

27TH STREET & 2ND AVENUE TURN LANE CONSTRUCTION PLANS



SHEET INDEX		
DRAWING	SHEET	DRAWING
G-1	1	COVER SHEET
LEG-1	2	LEGEND, ABBREVIATIONS, NOTES, & SUMMARY OF QUANTITIES
RMV - 1	3	REMOVAL PLANS
C - 1-2	4-5	CONSTRUCTION PLANS
MRK - 1	6	MARKING PLANS
DTL - 1-3	7-9	STANDARD DETAILS
303 - 1-4	10-13	CURB RAMP DETAILS

M. C. SCHAFF AND ASSOCIATES, INC.
818 SOUTH BELTLINE HIGHWAY EAST
SCOTTSBLUFF, NEBRASKA 69361

ENGINEERS ♦ PLANNERS ♦ DESIGNERS ♦ LAND SURVEYORS
 PH: 308-635-1926 FAX: 308-635-7807 INTERNET: WWW.MCSCHAFF.COM



LEGEND

SYMBOL	EXISTING	PROPOSED
PROPERTY CORNER		
RIGHT OF WAY MARKER		
STOP SIGN		
ROAD SIGN		
DELINEATOR		
BOLLARD		
MAILBOX		
FIRE HYDRANT		
VALVE		
METER PIT		
MANHOLE		
STORM INLET		
POWER POLE		
LIGHT POLE		
GUY WIRE		
ELECTRIC TRANSFORMER		
TRAFFIC SIGNAL W/ LUMINARY		
TELEPHONE PEDESTAL		
TELEPHONE MARKER		
GAS MARKER		
CABLE TV PEDESTAL		
DECIDUOUS TREE		
CONIFER		
MONITORING WELL		

LEGEND

SYMBOL	EXISTING	PROPOSED
RIGHT OF WAY		
CENTERLINE		
WATER MAIN		
6" WATER MAIN		
8" WATER MAIN		
12" WATER MAIN		
8" SANITARY SEWER		
12" SANITARY SEWER		
15" SANITARY SEWER		
STORM DRAIN		
OVERHEAD ELECTRIC		
UNDERGROUND ELECTRIC		
UNDERGROUND TELEPHONE		
UNDERGROUND CABLE TV		
UNDERGROUND FIBER OPTIC		
GAS MAIN		
FENCE		
EDGE OF PAVEMENT		
CURB AND GUTTER		
CONSTRUCTION LIMITS		

ABBREVIATIONS

ADA	AMERICANS WITH DISABILITIES ACT
AUX	AUXILIARY
BB	BACK TO BACK
BDC	BEGIN DROP CURB
BL	BURY LINE
BOP	BEGINNING OF PROJECT
C, C/L	CENTERLINE
CMP	CORRUGATED METAL PIPE
CY	CUBIC YARDS
D	DEGREE OF CURVATURE
DC	TOP OF DROP CURB
DIA	DIAMETER
E	EAST, EASTING
EA	EACH
EDC	END DROP CURB
EL, ELEV	ELEVATION
FES	FLARED END SECTION
FL	FLOW LINE
FT	FEET, FOOT
HOR	HORIZONTAL
IE	INVERT ELEVATION
LBS	POUNDS
LF	LINEAL FEET
LIN	LINEAL
LT	LEFT
MAX	MAXIMUM
MIN	MINIMUM
N	NORTH, NORTHING
OC	ON CENTER
OFF	OFFSET
P.C.	PORTLAND CEMENT
PC	POINT OF CURVATURE
PI	POINT OF INTERSECTION
PREP	PREPARATION
PT	POINT OF TANGENCY
PVC	POLYVINYL CHLORIDE
R, RAD	RADIUS
RCCP	REINFORCED CONCRETE CULVERT PIPE
RE:	REFERENCE
REINF	REINFORCE, REINFORCING
RGSV	RESILIENT SEATED GATE VALVE
ROW	RIGHT OF WAY
RT	RIGHT
SQ	SQUARE
STA	STATION
SY	SQUARE YARDS
T	TANGENT
TC	TOP OF CURB
TEMP	TEMPORARY
TP	TOP OF PAVEMENT
TW	TOP OF SIDEWALK
TYP	TYPICAL
VER	VERTICAL
VO	VERTICAL OPERATING
VPI	VERTICAL POINT OF INTERSECTION
YDS	YARDS

GENERAL NOTES

THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL REQUIRED PERMITS PRIOR TO COMMENCEMENT OF ANY WORK ON THE PROJECT.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY THE ENGINEER OF ANY PROBLEMS IN CONFORMING TO THE APPROVED PLANS FOR ANY ELEMENT OF THE PROPOSED IMPROVEMENTS PRIOR TO ITS CONSTRUCTION.

ANY CONSTRUCTION DEBRIS OR MUD TRACKING ONTO THE PUBLIC RIGHT OF WAY, RESULTING FROM THE PROJECT, SHALL BE REMOVED IMMEDIATELY, BY THE CONTRACTOR. THE CONTRACTOR SHALL IMMEDIATELY FIX ANY EXCAVATION, OR EXCESSIVE PAVEMENT FAILURE CAUSED BY THE FAILURE, AND SHALL PROPERLY BARRICADE THE SITE UNTIL CONSTRUCTION IS COMPLETE. FAILURE BY THE CONTRACTOR, TO CORRECT ANY OF THE ABOVE WITHIN 48 HOURS OF WRITTEN NOTICE, BY THE CITY, SHALL CAUSE THE CITY TO ISSUE A STOP WORK ORDER (RED TAG) AND/OR DO THE WORK AND MAKE A CLAIM AGAINST THE PROJECTS LETTER OF CREDIT FOR ANY COSTS INCURRED BY THE CITY OF SCOTTSBLUFF.

THE CONTRACTOR SHALL BE SOLELY, AND COMPLETELY RESPONSIBLE FOR THE CONDITIONS AT, AND ADJACENT TO THE JOB SITE, INCLUDING THE SAFETY OF ALL PERSONS AND PROPERTY, DURING THE PERFORMANCE OF THE WORK, THIS REQUIREMENT SHALL APPLY CONTINUOUSLY, AND SHALL NOT BE LIMITED TO NORMAL WORKING HOURS, THE DUTY OF THE VILLAGE TO CONDUCT CONSTRUCTION REVIEW OF THE CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO INCLUDE THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES IN, ON OR NEAR THE CONSTRUCTION SITE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING UTILITY LOCATIONS AT LEAST 48 HOURS (2 BUSINESS DAYS) PRIOR TO THE COMMENCEMENT OF ANY CONSTRUCTION. FOR INFORMATION CONTACT DIGGERS HOTLINE: 1-800-331-5666. THE CONTRACTOR SHALL FIELD VERIFY SIZE, HORIZONTAL AND VERTICAL LOCATIONS OF EXISTING FACILITIES PRIOR TO ANY CONSTRUCTION AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES.

THE CONTRACTOR SHALL PROTECT ALL EXISTING UTILITY SERVICES AND SHALL REPLACE ANY DAMAGED UTILITY AT HIS OWN EXPENSE. THE CONTRACTOR WILL ALSO BE RESPONSIBLE FOR COORDINATING ALL UTILITY RELOCATION WORK WITH THE APPROPRIATE UTILITY COMPANY DURING THE CONSTRUCTION PROCESS.

THE CONTRACTOR SHALL NOTIFY ALL UTILITY OWNERS PRIOR TO ADJUSTING ALL CLEANOUTS, MANHOLES, VALVE BOXES, SURVEY MONUMENTS, AND ANY OTHER FIXTURES PRIOR TO FINAL PAVING.

THE CONTRACTOR SHALL PROVIDE ALL LIGHTS, SIGNS, BARRICADES, FLAG PERSONS OR OTHER DEVICES NECESSARY TO PROVIDE FOR THE PUBLIC SAFETY IN ACCORDANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE CONTRACTOR SHALL PROVIDE INGRESS AND EGRESS TO ALL PROPERTIES ABUTTING THE PROJECT SITE THROUGHOUT THE PERIOD OF CONSTRUCTION. PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL CONTACT THE PROPERTY OWNERS IMPACTED BY THIS PROJECT.

PRIOR TO FINAL PLACEMENT OF PAVEMENT, ALL UNDERGROUND UTILITY MAINS SHALL BE INSTALLED AND SERVICE CONNECTIONS STUBBED OUT BEYOND THE CURB LINE, WHEN ALLOWED BY THE UTILITY. SERVICE BY THE PUBLIC UTILITIES AND FROM SANITARY SEWERS SHALL BE MADE AVAILABLE FOR EACH PROPERTY IN SUCH A MANNER THAT IT WILL NOT BE NECESSARY TO DISTURB THE STREET PAVEMENT, CURB, GUTTER AND SIDEWALK WHEN CONNECTIONS ARE MADE.

THE CONTRACTOR SHALL NOTIFY THE INSPECTOR AND TESTING AGENCY AT LEAST 24 HOURS PRIOR TO THE DESIRED INSPECTION.

ALL TRENCHES SHALL BE ADEQUATELY SUPPORTED AND THE SAFETY OF WORKERS PROVIDED FOR AS REQUIRED BY THE MOST RECENT OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION. THESE REGULATIONS ARE DESCRIBED IN SUBPART P., PART 1926 OF THE CODE OF FEDERAL REGULATIONS, UTILIZE SHEETING AND SHORING WHERE REQUIRED TO PREVENT ANY EXCESSIVE WIDENING OR SLOUGHING OF THE TRENCH WHICH MAY BE DETRIMENTAL TO HUMAN SAFETY, TO THE PIPE BEING PLACED, TO TREES, OR TO ANY EXISTING STRUCTURE, WHERE EXCAVATIONS ARE MADE UNDER SEVERE WATER CONDITIONS, THE CONTRACTOR MAY BE REQUIRED TO USE AN APPROVED PILING INSTEAD OF SHEETING AND SHORING. EXCAVATED MATERIAL SHALL NOT BE PLACED NEARER THAN 2 FEET FROM THE SIDES OF THE TRENCH. HEAVY EQUIPMENT SHALL NOT BE PLACED NEAR THE SIDES OF THE TRENCH UNLESS THE TRENCH IS ADEQUATELY BRACED.

COMPACTION OF ALL TRENCHES AND ANY OTHER REQUIRED AREAS MUST BE ATTAINED AND THE TEST RESULTS SUBMITTED TO THE ENGINEER AND THE CITY OF SCOTTSBLUFF PRIOR TO ACCEPTANCE.

THE CONTRACTOR SHALL HAVE IN HIS/HER POSSESSION AT ALL TIMES AT LEAST ONE COPY OF THE PLANS APPROVED BY THE CITY OF SCOTTSBLUFF.

THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND THE CITY OF SCOTTSBLUFF AT LEAST 48 HOURS PRIOR TO THE START OF ANY CONSTRUCTION.

THE CONTRACTOR IS RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING DUST CONTROL MEASURES AT ALL TIMES DURING CONSTRUCTION.

REPAIR OF ANY DAMAGE TO EXISTING IMPROVEMENTS OR LANDSCAPING IS THE RESPONSIBILITY OF THE CONTRACTOR AT NO EXPENSE TO THE OWNER.

QUANTITIES - 27TH STREET & 2ND AVENUE TURN LANE

ITEM	DESCRIPTION	ESTIMATED QUANTITY
1	MOBILIZATION	1 LUMP SUM
2	TRAFFIC CONTROL	1 LUMP SUM
3	REMOVE EXISTING PAVEMENT, CURB, GUTTER, DRIVEWAY, AND SIDEWALKS	3,900 S.F.
4	REMOVE STORM DRAIN INLET	1 EA.
5	REMOVE STORM DRAIN PIPE	10 L.F.
6	REMOVE EXISTING PAVEMENT MARKING	2,045 L.F.
7	8-INCH P.C. CONCRETE PAVEMENT	250 S.Y.
8	30-INCH P.C. CONCRETE CURB AND GUTTER	260 L.F.
9	30-FOOT INTERSECTION RADIUS	1 EA.
10	6-INCH P.C. CONCRETE DRIVEWAY	325 S.F.
11	4-INCH P.C. CONCRETE SIDEWALK	2,025 S.F.
12	HANDICAP RAMP	1 EA.
13	STANDARD STORM DRAIN INLET	2 EA.
14	STORM DRAIN JUNCTION BOX	2 EA.
15	12-INCH R.C.C.P. STORM DRAIN	85 L.F.
16	RELOCATE EXISTING FIRE HYDRANT AND VALVE	1 EA.
17	ADJUST VALVE BOX TO GRADE	1 EA.
18	ADJUST PULL BOX TO GRADE	1 EA.
19	ADJUST MANHOLE TO GRADE	1 EA.
20	4-INCH WHITE WET REFLECTIVE PREFORMED PAVEMENT MARKING, TYPE 4, GROOVED	210 L.F.
21	4-INCH YELLOW WET REFLECTIVE PREFORMED PAVEMENT MARKING, TYPE 4, GROOVED	1,600 L.F.
22	6-INCH WHITE WET REFLECTIVE PREFORMED PAVEMENT MARKING, TYPE 4, GROOVED	385 L.F.
23	12-INCH YELLOW WET REFLECTIVE PREFORMED PAVEMENT MARKING, TYPE 4, GROOVED	210 L.F.
24	24-INCH WHITE PREFORMED PAVEMENT MARKING, TYPE 4, GROOVED	15 L.F.
25	WHITE LEFT TURN ARROW WET REFLECTIVE PREFORMED PAVEMENT MARKING, TYPE 4, GROOVED	4 EA.
26	SODDING	905 S.F.

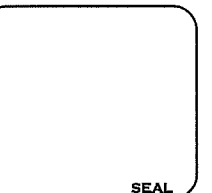
M. C. SCHAFF & ASSOCIATES, INC.
818 SOUTH BELTLINE HIGHWAY EAST
SCOTTSBLUFF, NEBRASKA 69361

ENGINEERS ♦ PLANNERS ♦ DESIGNERS ♦ LAND SURVEYORS
 PH: 308-635-1926 FAX: 308-635-7807 INTERNET: WWW.MCSCHAFF.COM

PROJECT: 27TH STREET AND SECOND AVENUE TURN LANE CONSTRUCTION PROJECT SCOTTSBLUFF, NEBRASKA LEGEND/SUMMARY OF QNTYS

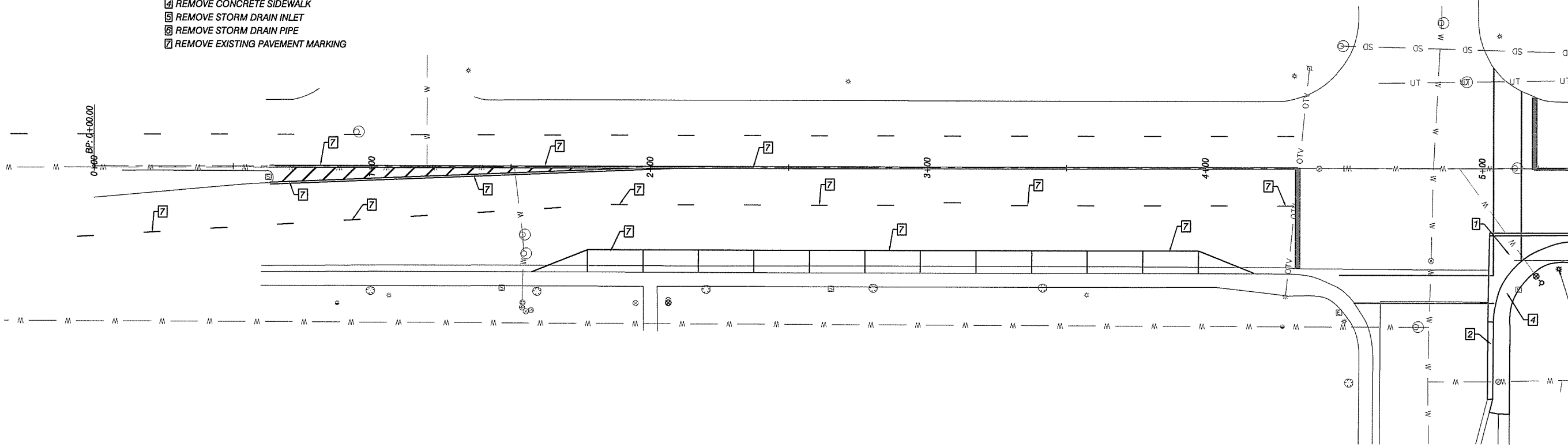
CLIENT: CITY OF SCOTTSBLUFF

PROJECT NUMBER: RM 14023400
PROJECT DATE: 1/15/2015
PROJECT MGR: D.J.D.
PROJECT TEAM: A.V./D.B./A.B.

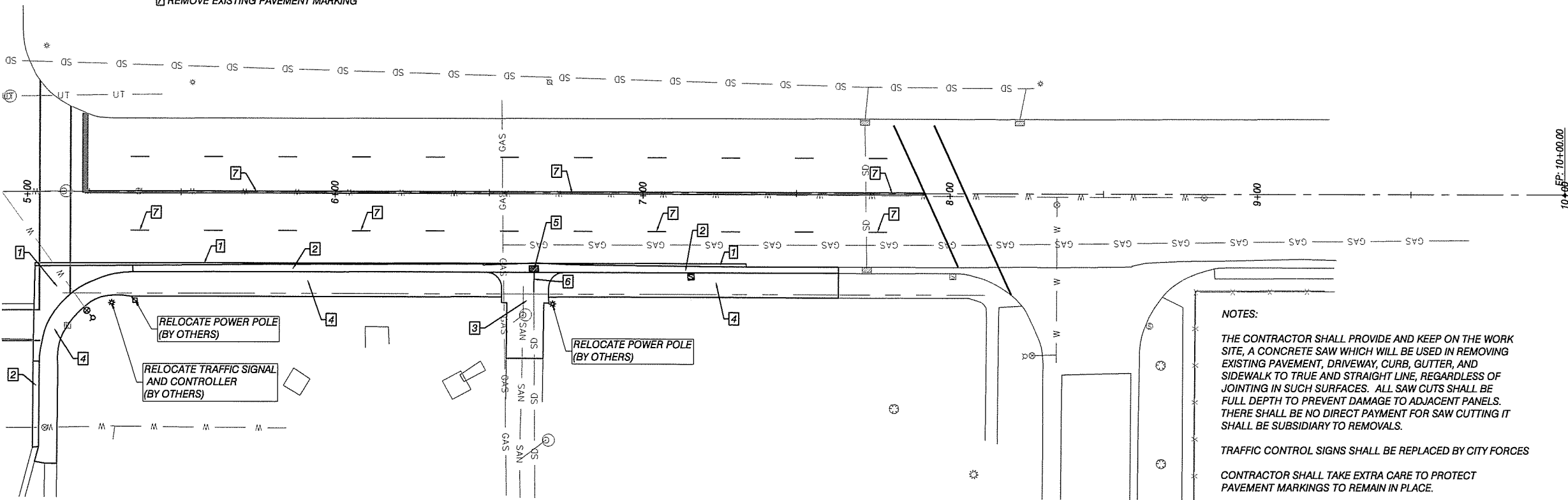


DATE	REVISION

- 1 REMOVE EXISTING PAVEMENT
- 2 REMOVE CONCRETE CURB AND GUTTER
- 3 REMOVE CONCRETE DRIVEWAY
- 4 REMOVE CONCRETE SIDEWALK
- 5 REMOVE STORM DRAIN INLET
- 6 REMOVE STORM DRAIN PIPE
- 7 REMOVE EXISTING PAVEMENT MARKING



- 1 REMOVE EXISTING PAVEMENT
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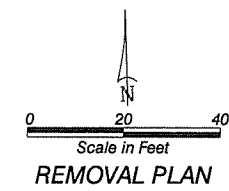


NOTES:

THE CONTRACTOR SHALL PROVIDE AND KEEP ON THE WORK SITE, A CONCRETE SAW WHICH WILL BE USED IN REMOVING EXISTING PAVEMENT, DRIVEWAY, CURB, GUTTER, AND SIDEWALK TO TRUE AND STRAIGHT LINE, REGARDLESS OF JOINTING IN SUCH SURFACES. ALL SAW CUTS SHALL BE FULL DEPTH TO PREVENT DAMAGE TO ADJACENT PANELS. THERE SHALL BE NO DIRECT PAYMENT FOR SAW CUTTING IT SHALL BE SUBSIDIARY TO REMOVALS.

TRAFFIC CONTROL SIGNS SHALL BE REPLACED BY CITY FORCES

CONTRACTOR SHALL TAKE EXTRA CARE TO PROTECT PAVEMENT MARKINGS TO REMAIN IN PLACE.



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PROJECT: 27TH STREET AND
SECOND AVENUE TURN LANE
CONSTRUCTION PROJECT
SCOTTSBLUFF, NEBRASKA
REMOVAL PLANS

CLIENT: CITY OF SCOTTSBLUFF

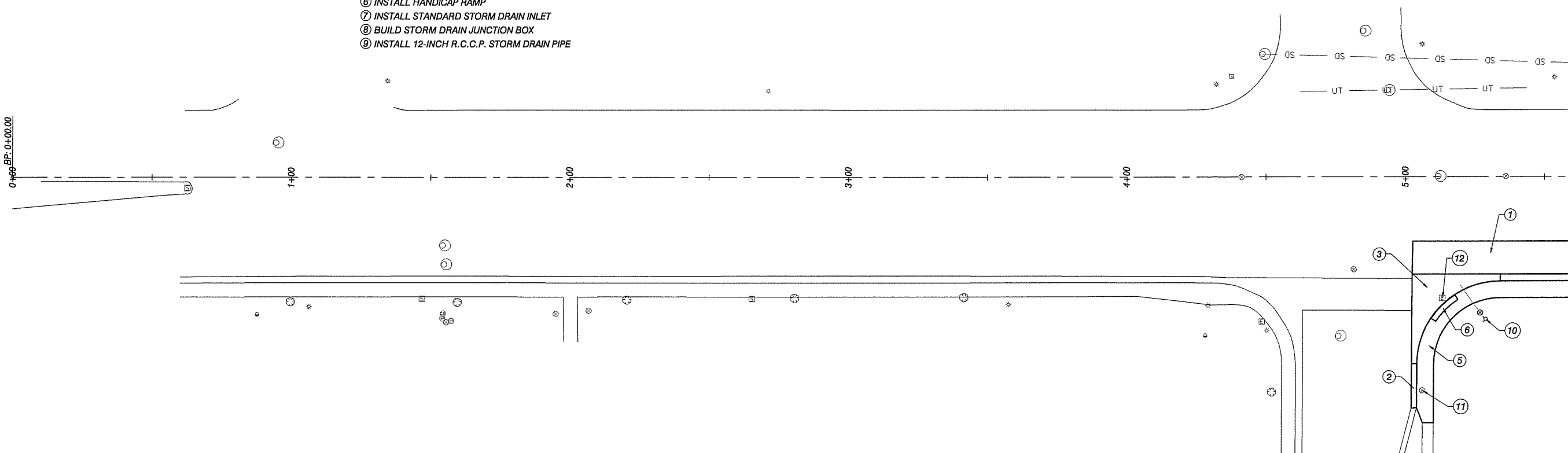
PROJECT NUMBER:	RM140234-00
PROJECT DATE:	1/15/2015
PROJECT MGR:	D.J.D.
PROJECT TEAM:	A.V./D.B./A.B.

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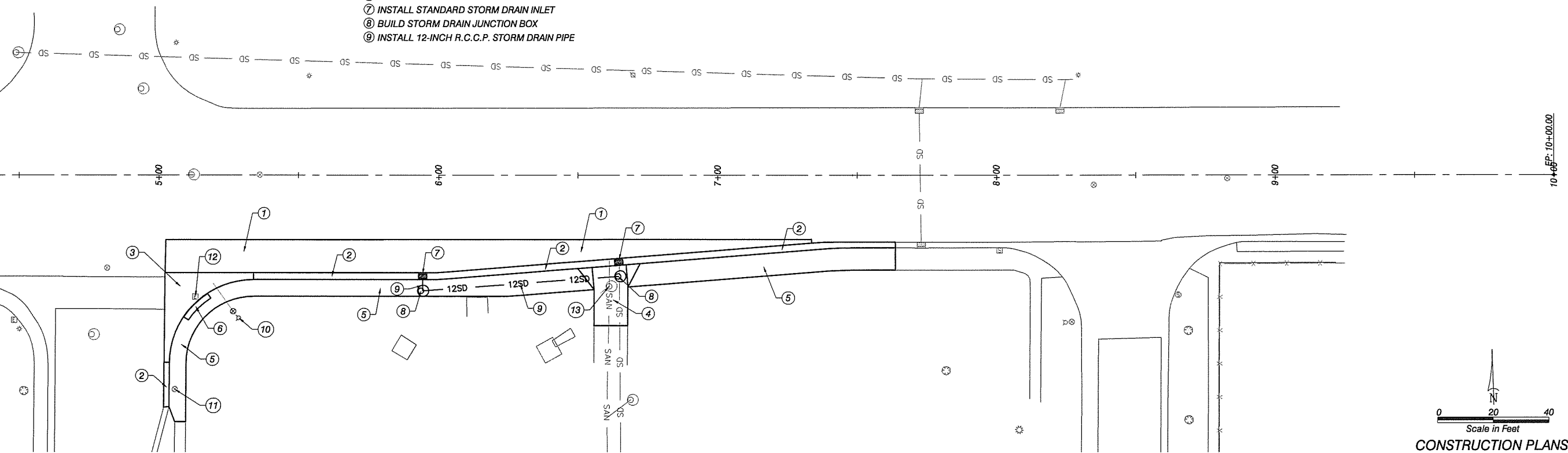
DATE	REVISION

SHEET 3 OF 13
RMV-1

- ① INSTALL 8-INCH P.C. CONCRETE PAVEMENT
- ② INSTALL 30-INCH P.C. CONCRETE CURB AND GUTTER
- ③ INSTALL 30-FOOT INTERSECTION RADIUS
- ④ INSTALL 6-INCH P.C. CONCRETE DRIVEWAY
- ⑤ INSTALL 4-INCH P.C. CONCRETE SIDEWALK
- ⑥ INSTALL HANDICAP RAMP
- ⑦ INSTALL STANDARD STORM DRAIN INLET
- ⑧ BUILD STORM DRAIN JUNCTION BOX
- ⑨ INSTALL 12-INCH R.C.C.P. STORM DRAIN PIPE
- ⑩ RELOCATE EXISTING FIRE HYDRANT AND VALVE
- ⑪ ADJUST VALVE BOX TO GRADE
- ⑫ ADJUST PULL BOX TO GRADE
- ⑬ ADJUST MANHOLE TO GRADE



- ① INSTALL 8-INCH P.C. CONCRETE PAVEMENT
- ② INSTALL 30-INCH P.C. CONCRETE CURB AND GUTTER
- ③ INSTALL 30-FOOT INTERSECTION RADIUS
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CONSTRUCTION PROJECT
SCOTTSBLUFF, NEBRASKA
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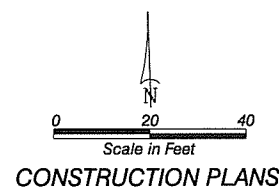
CLIENT: CITY OF SCOTTSBLUFF

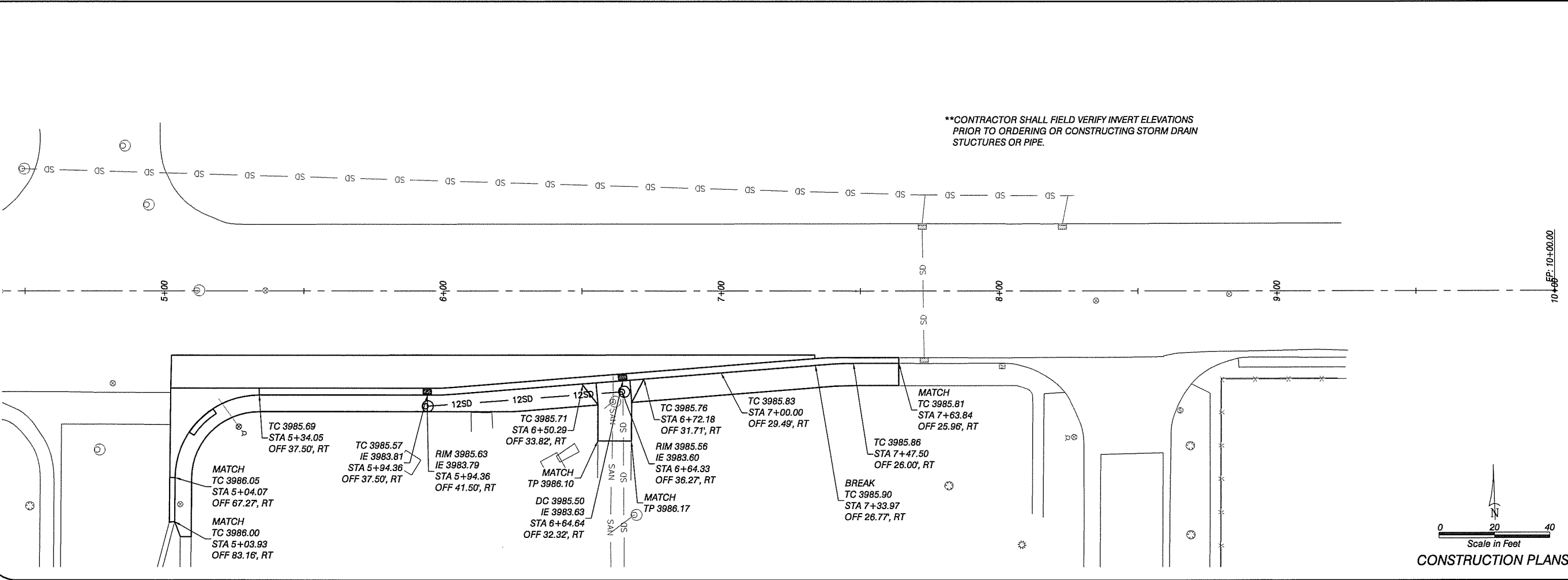
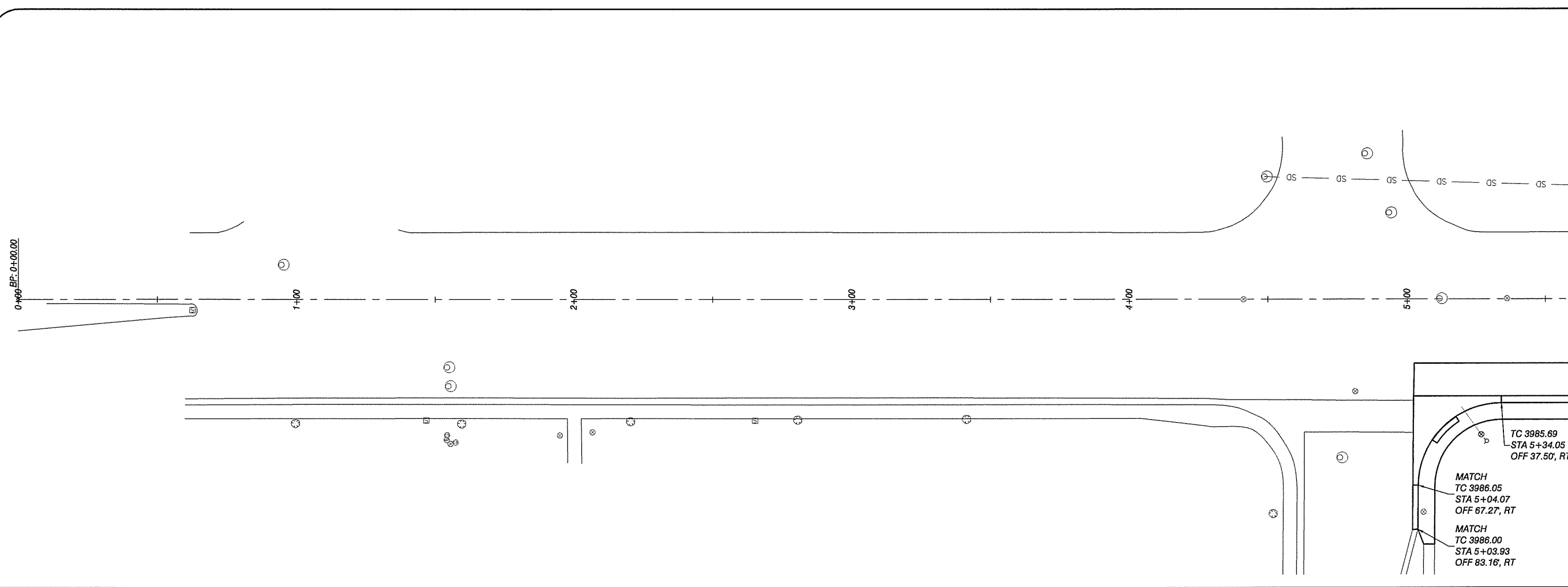
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PROJECT TEAM:
 A.V./D.B./A.B.

SEAL

DATE	REVISION

SHEET 4 OF 13
C-1





**CONTRACTOR SHALL FIELD VERIFY INVERT ELEVATIONS PRIOR TO ORDERING OR CONSTRUCTING STORM DRAIN STRUCTURES OR PIPE.

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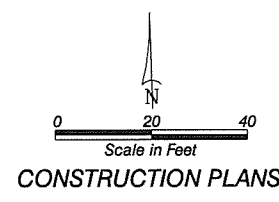
CLIENT: CITY OF SCOTTSBUFF

PROJECT NUMBER:	RM 140234-00
PROJECT DATE:	1/15/2015
PROJECT MGR:	D.J.D.
PROJECT TEAM:	A.V./D.B./A.B.

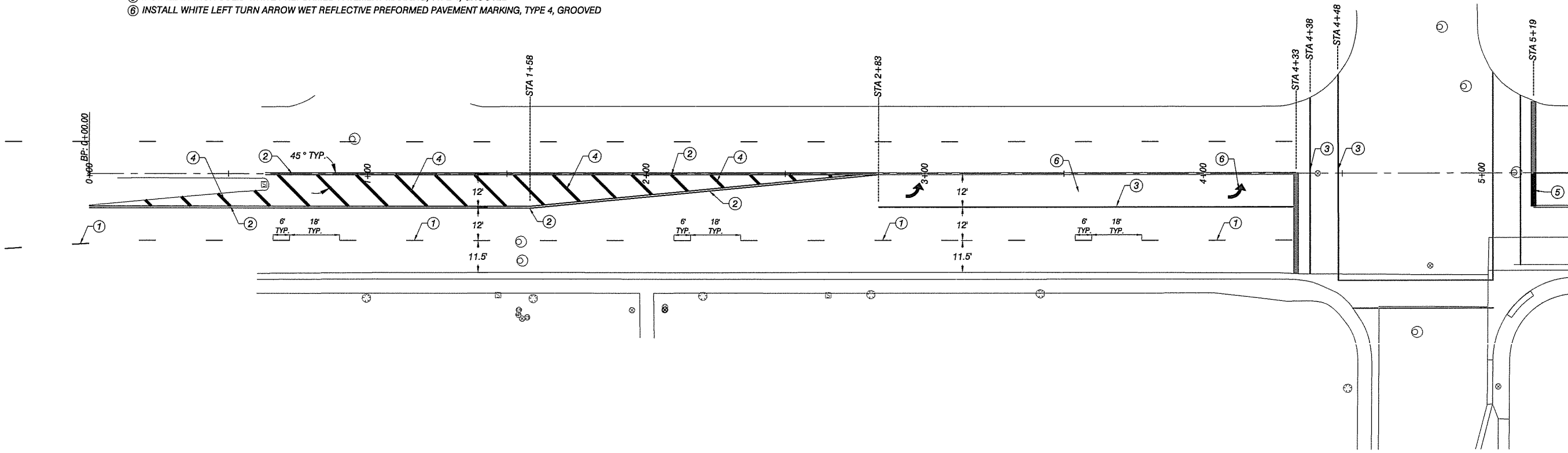
SEAL

DATE	REVISION

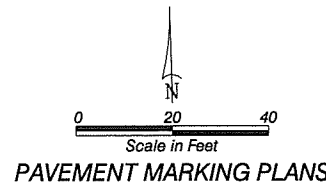
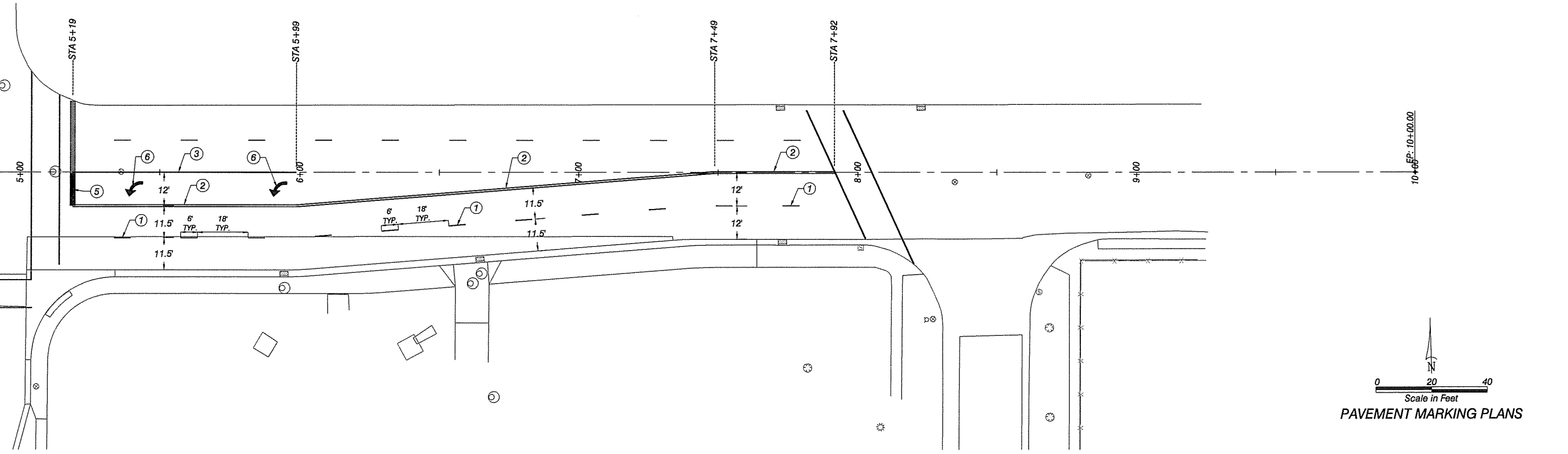
SHEET 5 OF 13



- ① INSTALL 4-INCH DASHED WHITE WET REFLECTIVE PREFORMED PAVEMENT MARKING, TYPE 4, GROOVED
- ② INSTALL DOUBLE 4-INCH SOLID YELLOW WET REFLECTIVE PREFORMED PAVEMENT MARKING, TYPE 4, GROOVED
- ③ INSTALL 6-INCH SOLID WHITE WET REFLECTIVE PREFORMED PAVEMENT MARKING, TYPE 4, GROOVED
- ④ INSTALL 12-INCH SOLID YELLOW WET REFLECTIVE PREFORMED PAVEMENT MARKING, TYPE 4, GROOVED
- ⑤ INSTALL 24-INCH SOLID WHITE PREFORMED PAVEMENT MARKING, TYPE 4, GROOVED
- ⑥ INSTALL WHITE LEFT TURN ARROW WET REFLECTIVE PREFORMED PAVEMENT MARKING, TYPE 4, GROOVED



- ① INSTALL 4-INCH DASHED WHITE WET REFLECTIVE PREFORMED PAVEMENT MARKING, TYPE 4, GROOVED
- ② INSTALL DOUBLE 4-INCH SOLID YELLOW WET REFLECTIVE PREFORMED PAVEMENT MARKING, TYPE 4, GROOVED
- ③ INSTALL 6-INCH SOLID WHITE WET REFLECTIVE PREFORMED PAVEMENT MARKING, TYPE 4, GROOVED
- ④ INSTALL 12-INCH SOLID YELLOW WET REFLECTIVE PREFORMED PAVEMENT MARKING, TYPE 4, GROOVED
- ⑤ INSTALL 24-INCH SOLID WHITE PREFORMED PAVEMENT MARKING, TYPE 4, GROOVED
- ⑥ INSTALL WHITE LEFT TURN ARROW WET REFLECTIVE PREFORMED PAVEMENT MARKING, TYPE 4, GROOVED



PAVEMENT MARKING PLANS

M. C. SCHAFF & ASSOCIATES, INC.
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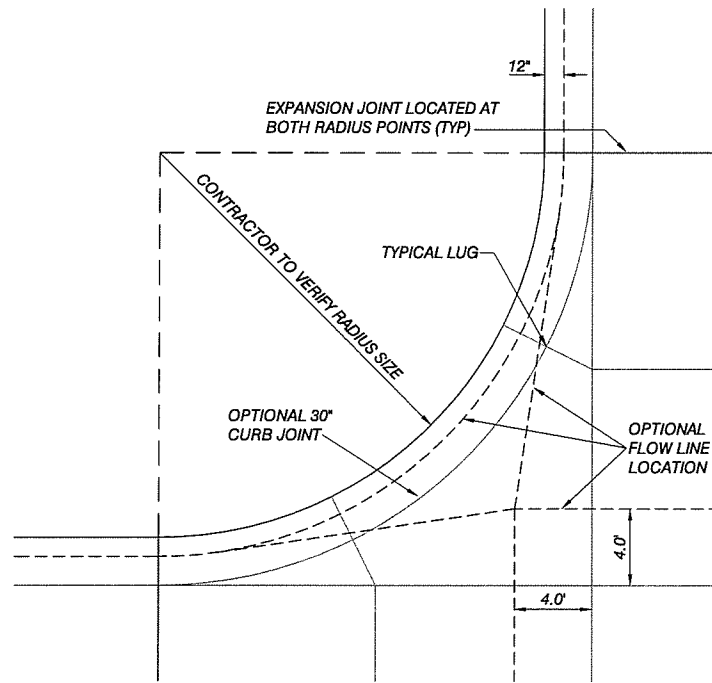
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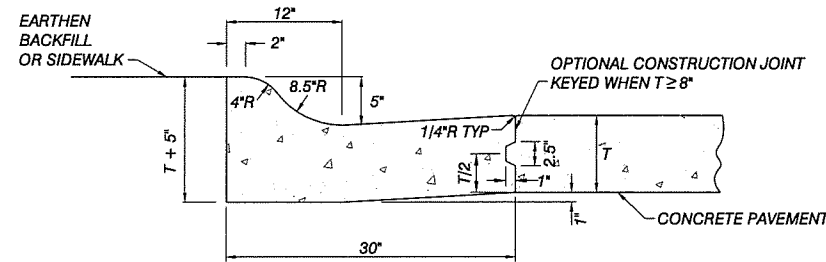
DATE	REVISION

SHEET 6 OF 13
MRK-1

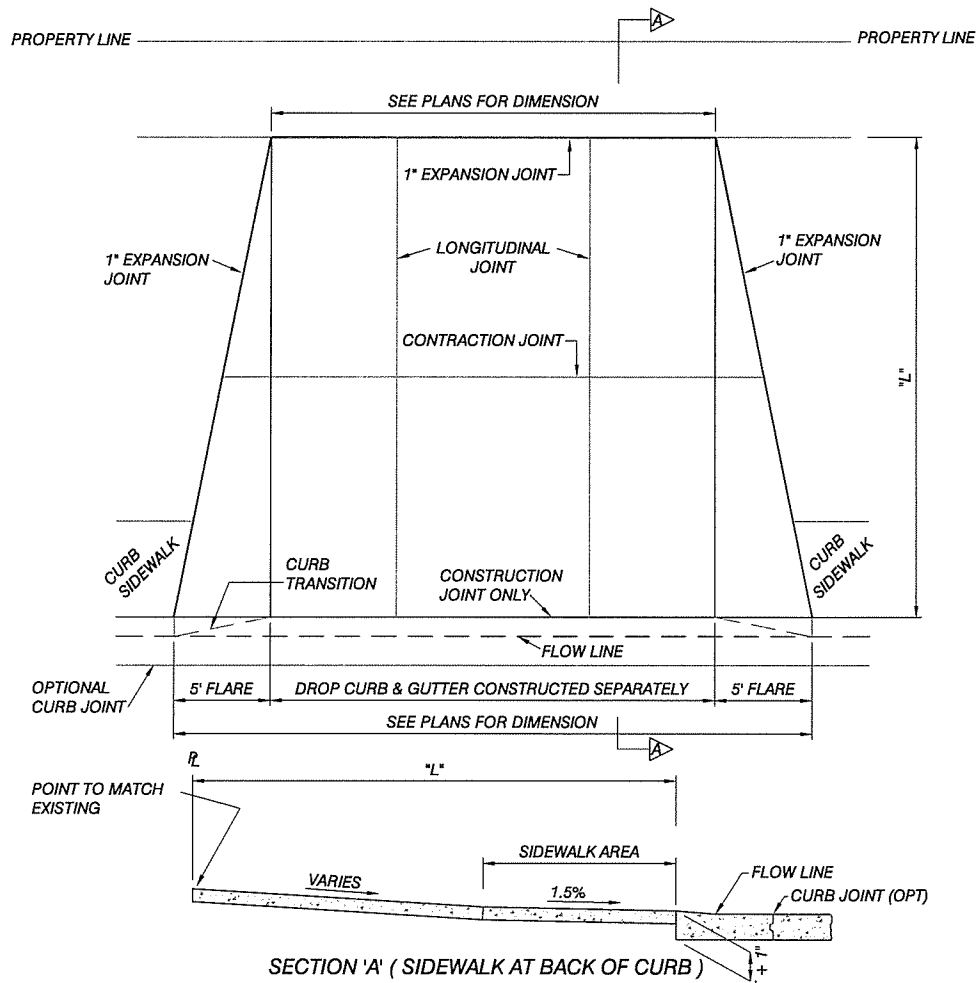


PAY ITEM FOR TYPICAL RADIUS SHALL INCLUDE THE CURB AND GUTTER, FILLET AND DROP CURB FOR CURB RAMP

TYPICAL CONCRETE INTERSECTION RADIUS



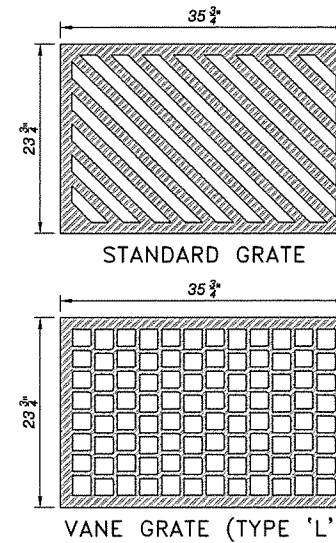
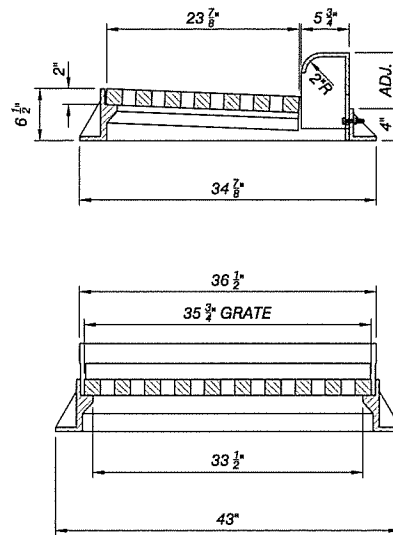
CONCRETE CURB IN PUBLIC RIGHT OF WAY



SEE PAVING PLAN FOR DRIVEWAY WIDTH AND DEPTH DIMENSIONS.

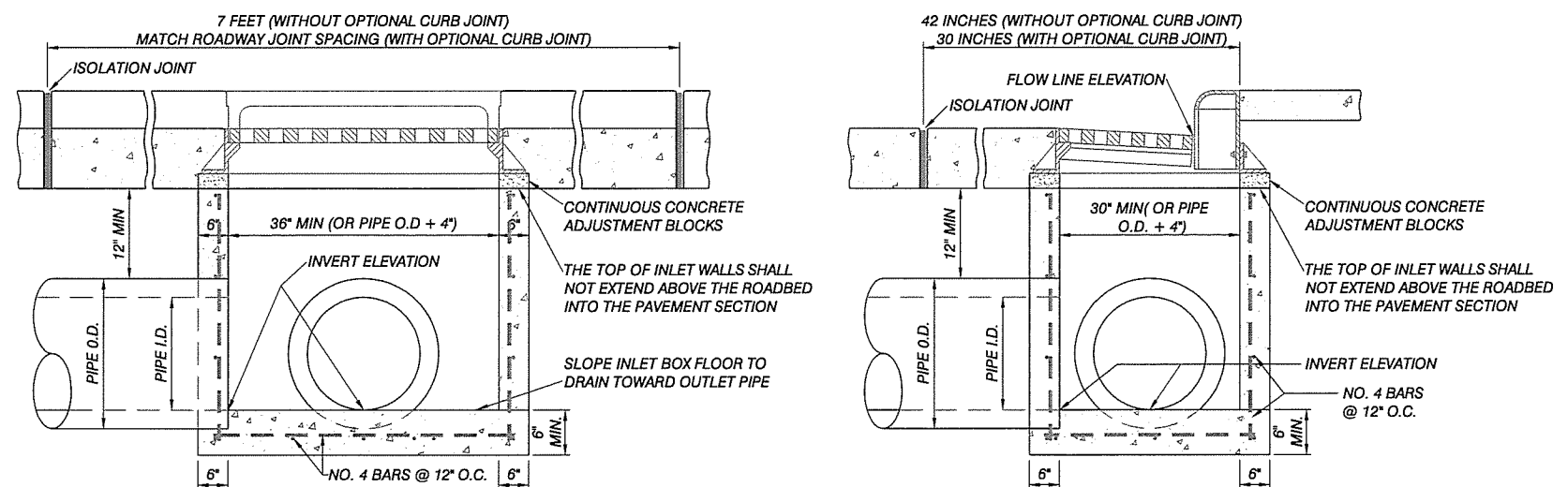
DROP CURB AND GUTTER SHALL BE INSTALLED SEPERATELY AND ARE NOT A PART OF THIS BID ITEM

TYPICAL DRIVEWAY/ALLEY RETURN



UNLESS SPECIFIED, THE STANDARD GRATE DESIGN SHALL BE SUPPLIED. THE CURB FACE SHALL BE A SEMI-ROLL WITH A 2 INCH RADIUS UNLESS OTHERWISE SPECIFIED. THE FRAME CURB BOX AND STANDARD GRATE SHALL BE NEENAH MODEL R-3246-A OR AN APPROVED EQUAL. IF THE STANDARD GRATE IS NOT TO BE USED THE PLANS SHALL SPECIFY WHICH GRATE SHALL BE USED.

TYPICAL CURB INLET, FRAME, GRATE AND CURB BOX



STANDARD CONCRETE CURB INLET BOX

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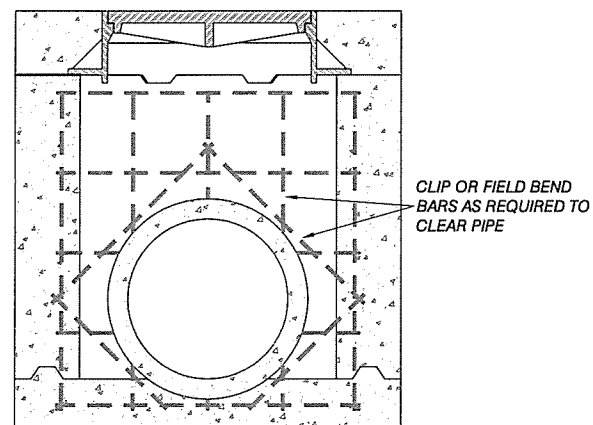
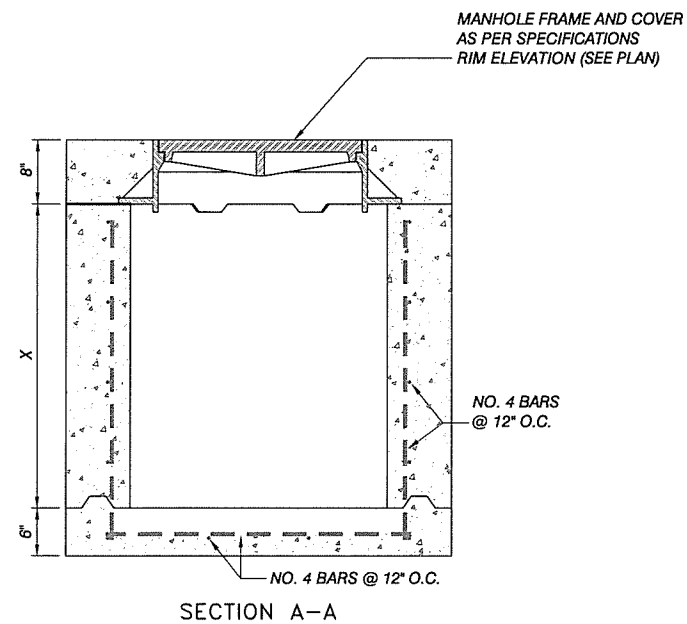
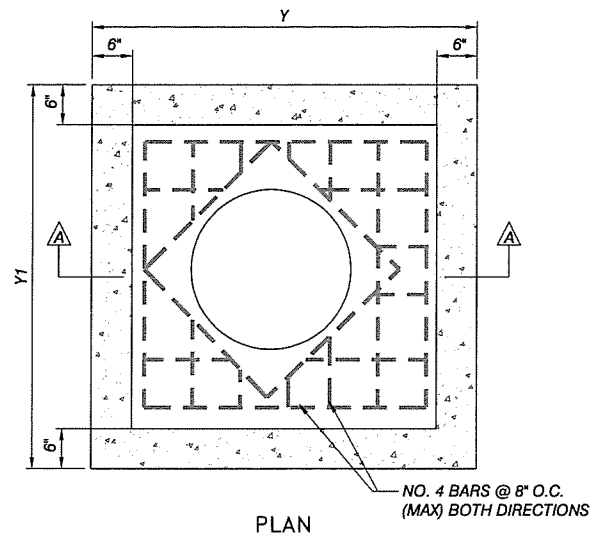
PROJECT: 27TH STREET AND
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CONSTRUCTION PROJECT
SCOTTSBLUFF, NEBRASKA
STANDARD DETAILS

CLIENT: CITY OF SCOTTSBLUFF

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PROJECT TEAM: A.V./D.B./A.B.

DATE	REVISION

SHEET 7 OF 13
DTL-1



DETAIL OF PIPE OPENING

STORM DRAIN JUNCTION BOX

NOTES

THE MINIMUM COVER MEASURED FROM THE FACE OF CONCRETE TO THE SURFACE OF ANY REINFORCING BAR SHALL BE 2 INCHES (EXCEPT AS SHOWN).

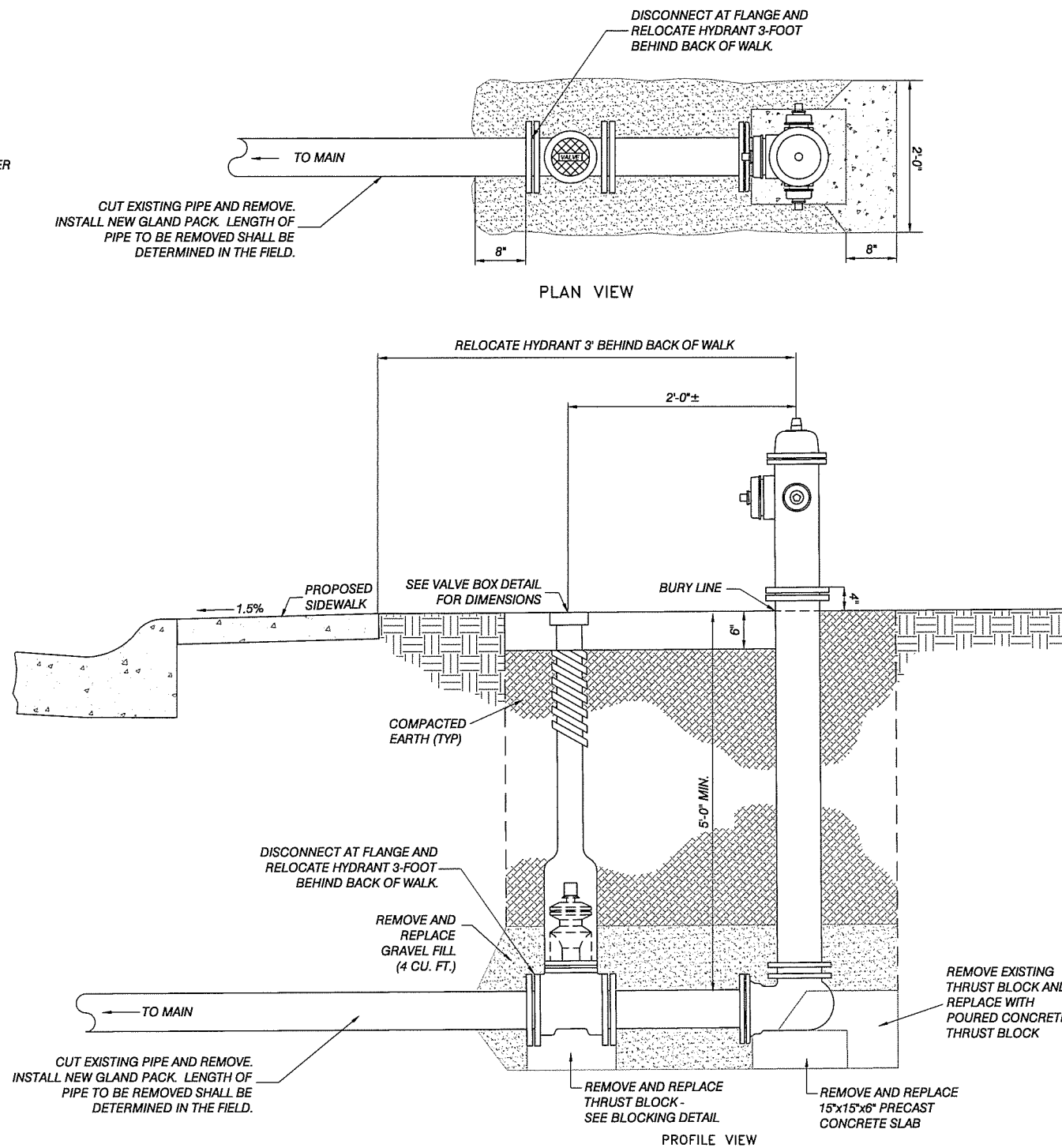
JUNCTION BOX SHALL BE PAID FOR AT THE UNIT PRICE PER EACH FOR THE ITEM "JUNCTION BOX". THIS PRICE SHALL INCLUDE THE RING AND COVER, THE FRAME IF REQUIRED, ALL NECESSARY EXCAVATION, CONCRETE, REINFORCING STEEL, MISCELLANEOUS MATERIALS AND LABOR.

DIMENSION "X" MAY BE INCREASED OR DECREASED FROM THAT SHOWN ON THE PLANS BY NOT MORE THAN SIX INCHES WITHOUT ANY ADJUSTMENT IN COMPENSATION. CHANGES IN EXCESS OF SIX INCHES SHALL BE PAID FOR ON AN EXTRA WORK BASIS.

RING AND COVER SHALL BE PLACED AS FOLLOWS:

- IN PAVED AREAS:
 - FLUSH WITH FINISHED PAVEMENT GRADE
 - IN UNPAVED TRAFFIC AREAS:
 - 3" BELOW FINISHED GRADE
 - IN ALL OTHER AREAS:
 - AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER

JUNCTION BOX RING AND COVER SHALL BE NEENAH MODEL NO. R-1769-A, DEETER MODEL NO. 1048 OR AN APPROVED EQUAL. SHOULD FLUSH MANHOLES BE REQUIRED, THEY SHALL BE NEENAH MODEL NO. R-8052-F, DEETER MODEL NO. 1054 OR AN APPROVED EQUAL.



FIRE HYDRANT RELOCATION/SETTING DETAIL
NOT TO SCALE

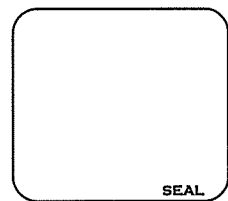
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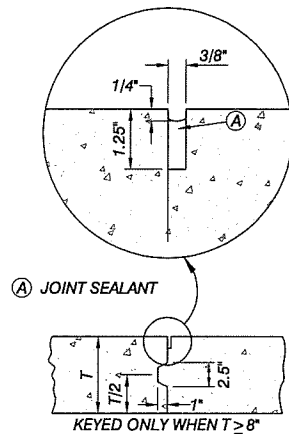
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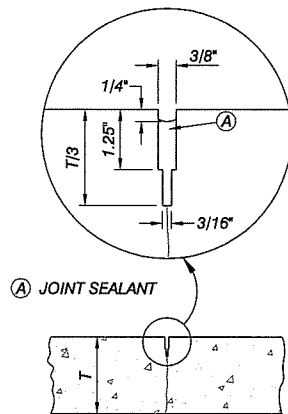
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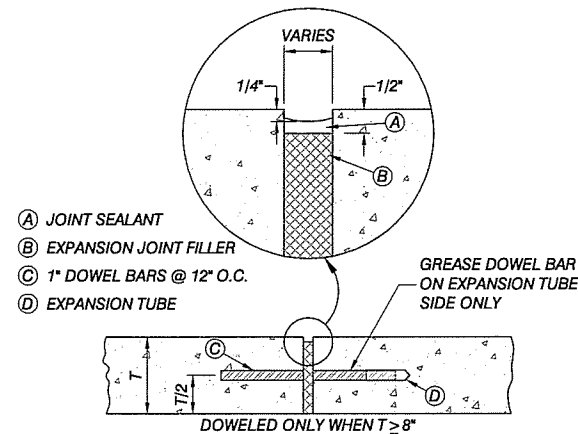
DATE	REVISION



TYPICAL TRANSVERSE ROADWAY CONSTRUCTION JOINT



TYPICAL SAWN TRANSVERSE ROADWAY CONTRACTION JOINT



TYPICAL EXPANSION JOINT

CONCRETE JOINTING NOTES

CONCRETE PAVEMENT JOINTS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE SUBSIDIARY TO THE PAVEMENT.

TRANSVERSE ROADWAY JOINTS SHALL BE SPACED AT 12 FEET EXCEPT AT LOCATIONS SUCH AS UTILITY FIXTURES AND INTERSECTION RADII WHERE TRANSVERSE JOINT SPACING MAY VARY FROM 9.5 TO 14.5 FEET.

TRANSVERSE CURB AND GUTTER JOINTS SHALL MATCH THE TRANSVERSE ROADWAY JOINTS.

LONGITUDINAL ROADWAY JOINTS SHALL EVENLY DIVIDE THE WIDTH OF THE ROADWAY, WITH A SPACING OF BETWEEN 9.5 AND 14.5 FEET. AS AN OPTION, A LONGITUDINAL CONSTRUCTION JOINT MAY BE PLACED 30 INCHES FROM THE BACK OF CURB. WHERE APPLICABLE, LONGITUDINAL JOINTS SHALL NOT BE PLACED WHERE THEY WILL DIRECTLY COINCIDE WITH ANY LONGITUDINAL PAVEMENT MARKINGS. INSTEAD, LONGITUDINAL JOINTS SHALL BE PLACED SUCH THAT THERE IS A 1 TO 2 INCH GAP BETWEEN THE JOINT AND THE EDGE OF THE PAVEMENT MARKING.

DRIVEWAY, SIDEWALK AND MEDIAN JOINTS SHALL RESULT IN RECTANGULAR PANELS WITH EACH EDGE MEASURING BETWEEN 12 AND 24 TIMES THE SLAB THICKNESS. ADDITIONALLY, THE LENGTH ANY PANEL SHALL NOT EXCEED 1.5 TIMES THE WIDTH.

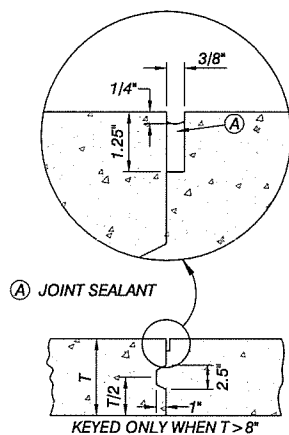
TRANSVERSE ROADWAY, DRIVEWAY, SIDEWALK AND MEDIAN JOINTS SHALL BE PERPENDICULAR TO BOTH THE SURFACE OF THE SUBGRADE AND THE DIRECTION OF TRAVEL.

IN GENERAL, ALL JOINTS SHALL INTERSECT AT 90° ANGLES, EXCEPT AT LOCATIONS SUCH AS UTILITY FIXTURES AND INTERSECTION RADII. WHEREVER JOINTS (IF EXTENDED) WOULD INTERSECT AT AN ANGLE OF LESS THAN 60°, A DOG LEG OF LUG SHALL BE CONSTRUCTED AS SHOWN IN THE DETAIL.

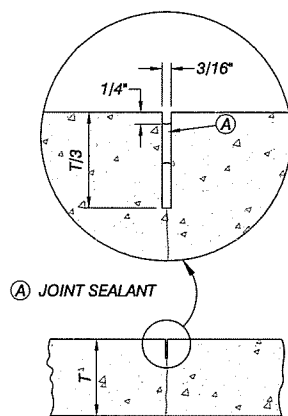
ROADWAY EXPANSION JOINTS SHALL BE CONTINUOUS FROM BACK OF CURB TO BACK OF CURB, AND CONTINUOUS THROUGH MEDIANS. ROADWAY EXPANSION JOINTS SHALL BE SPACED AT INTERVALS NOT TO EXCEED 300 FEET, AND JUST PRIOR TO ALL ROADWAY INTERSECTIONS, OR AS SHOWN ON THE PAVING PLANS. EXPANSION JOINTS OUTSIDE THE ROADWAY (INCLUDING MEDIANS) SHALL BE PLACED AT INTERVALS NOT TO EXCEED 100 FEET, OR AS SHOWN IN THE DETAILS OR ON THE PAVING PLANS. EXPANSION JOINTS WITHIN THE ROADWAY SHALL HAVE DOWEL BARS AS SHOWN IN THE DETAILS. DOWEL BARS SHALL BE SUPPORTED AT MID-DEPTH OF THE SLAB AT ALL EXPANSION JOINTS BY THE USE OF CHAIRS AND SPACER BARS. CHAIRS SHALL BE PLACED NO FURTHER THAN 30 INCHES ON CENTER WITH THREE NO. 4 SPACER BARS. ALL DOWEL BARS SHALL BE WIRED TO ALL SPACER BARS. ROADWAY EXPANSION JOINTS WITH DOWEL BARS SHALL BE PLACED PERPENDICULAR TO BOTH THE ROADWAY AND ROADBED. DOWEL BARS SHALL BE SECURED PARALLEL TO BOTH THE ROADWAY AND ROADBED. EXPANSION JOINTS OUTSIDE THE ROADWAY SHALL NOT HAVE DOWEL BARS. EXPANSION JOINT FILLER SHALL BE 1 INCH THICK FOR ALL EXPANSION JOINTS PERPENDICULAR TO THE ROADWAY, AND 1/2 INCH THICK WHERE PLACED PARALLEL WITH THE ROADWAY. THE DEPTH OF THE EXPANSION JOINT FILLER SHALL BE 1/2 INCH LESS THAN THAT OF THE THINNEST ADJACENT PAVEMENT.

ISOLATION JOINTS SHALL BE PLACED BETWEEN ANY STRUCTURES, SUCH AS MANHOLES, INLETS, LIGHT POLES AND THE ADJACENT PAVEMENT. ALL ISOLATION JOINTS SHALL BE 1/2 INCH THICK AND PLACED AS SHOWN IN THE DETAILS OR ON THE PAVING PLANS.

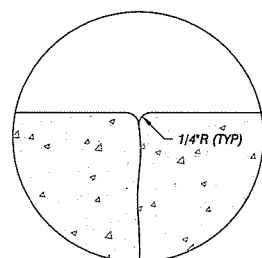
ALL ROADWAY JOINTS SHALL BE SEALED WITH A JOINT SEALANT THAT MEETS THE REQUIREMENTS SET FORTH IN THE SPECIFICATIONS FOR THIS PROJECT. JOINTS OUTSIDE THE FLOW LINE OF THE CURB, NEED NOT BE SEALED, INCLUDING MEDIAN JOINTS.



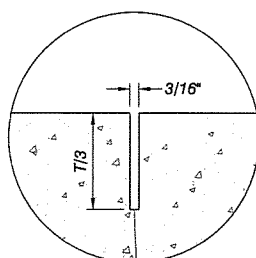
TYPICAL LONGITUDINAL ROADWAY CONSTRUCTION JOINT



TYPICAL SAWN LONGITUDINAL ROADWAY CONTRACTION JOINT



A TOOLED JOINT SHALL HAVE A WEAKENED PLANE INDUCED AT THE JOINT BY SLICING THE PAVEMENT ALONG THE JOINT WITH A MASON'S TROWEL (OR OTHER SUITABLE METHOD)
TYPICAL TOOLED CURB, SIDEWALK, DRIVEWAY OR MEDIAN JOINT



TYPICAL SAWN CURB, SIDEWALK, DRIVEWAY OR MEDIAN JOINT

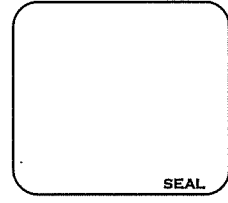
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