



City of Grand Island

Tuesday, March 25, 2014

Council Session (Corrected)

Item F-3

#9480 - Consideration of Amending Chapter 33 of the Grand Island City Code Relative to Subdivision Regulations – Street Design Standards

This item relates to the aforementioned Public Hearing item E-3.

Staff Contact: Chad Nabity

ORDINANCE NO. 9480

An ordinance to amend Chapter 33 Section 12 of the Grand Island City Code; to amend Chapter 33 Section 12 pertaining to Standards for Streets and Alleys; to repeal Chapter 33 Section 12 as now existing, and any ordinance or parts of ordinances in conflict herewith; and to provide for publication and the effective date of this ordinance.

BE IT ORDAINED BY THE MAYOR AND COUNCIL OF THE CITY OF GRAND ISLAND, NEBRASKA:

SECTION 1. Sections 33-12 of the Grand Island City Code are hereby amended to read as follows:

§33-12. Streets and Alleys

(1) The arrangement of streets shall conform as nearly as possible to the street plan of the General Development Plan with provisions for the extension of arterial and collector streets. Streets in the subdivision, normally shall connect with streets already dedicated in adjoining or adjacent subdivisions, and provisions may be required for future connections to adjoining unsubdivided tracts.

(2) Local streets should be so planned as to discourage through traffic. Cul-de-sacs should normally not be longer than five hundred feet and shall terminate with right-of-way turn-around having a diameter of not less than one hundred feet, and an outside curb diameter of not less than eighty feet for residential areas. Cul-de-sacs within industrial or commercial areas shall have a right-of-way diameter of not less than 120 feet and an outside curb diameter of not less than 100 feet.

(3) Collector and Arterial streets should be planned with minimal local street and driveway accesses. Residential subdivisions should be designed with street patterns that provide driveway access from local streets.

(4) Whenever a proposed subdivision is adjacent to or contains a portion of an existing or proposed federal or state highway, provision in such subdivision shall be made for one of the following methods of development:

(a) If the highway is either a non-access or controlled access thoroughfare, one of the following two methods of development shall be required:

(i) A frontage street adjacent and parallel to such thoroughfare shall be provided; or

(ii) Lots shall back or side to such thoroughfare and have access to another street. Lots in commercial or industrial zoning districts shall have a landscaped area averaging thirty feet in width, or other approved landscaping, adjacent to such thoroughfare, and outside storage of unassembled or unfinished materials or products and inoperable equipment or motor vehicles shall be suitably screened by a sight-obscuring fence, foliage, or other screening material. Lots in residential zoning districts shall have a sight-obscuring fence, foliage, or other screening material adjacent to such thoroughfare. The sight-obscuring fence shall not be metal strips or slats in a chain link fence.

(b) If the highway is not a non-access or controlled access thoroughfare, one of the following methods of development shall be required:

(i) Either method required for a non-access or controlled access thoroughfare may be applied; or

(ii) Lots may have frontage directly on such thoroughfare, provided, that the minimum setback for any new building in any zoning district shall be thirty feet, and shall be landscaped except for approved driveways.

| | |
|---------------------|-----------------|
| Approved as to Form | ☐ _____ |
| March 21, 2014 | ☐ City Attorney |

ORDINANCE NO. 9480 (Cont.)

(5) Half streets shall be prohibited except where essential to the reasonable development of the subdivision in conformity with the other requirements of these regulations, or where it is found to be practicable to require the dedication of the other half when adjoining property is subdivided.

(6) Under normal conditions streets shall be laid out as to intersect as nearly as possible at right angles, except where topography or other conditions justify variations. More than four approaches to any intersection shall be prohibited. Street jogs at intersections with centerline offsets of less than 75 feet should be avoided.

(7) Alleys may be provided in commercial and industrial districts. Alleys shall be avoided in residential districts except to extend existing alleys to a street.

(8) The right-of-way widths, improvements, and grades for streets and alleys included in any subdivision shall not be less than the minimum right-of-way and outside curb dimensions or less than the minimum grade for each classification as follows shown on the street cross sections as recommended by the Grand Island Public Works Department and approved by the Grand Island City Council attached as addendum C-Approved Street Cross Sections.

| Residential Areas | | | |
|----------------------------------|--------|----------|-------|
| | R.O.W. | Improved | Grade |
| Arterial Street Five Lane Design | 100' | 65'* | 0.3% |
| Arterial Street | 80 | 47'* | 0.3% |
| Collector Street | 60' | 41'* | 0.3% |
| Local Street | 60' | 37' | 0.3% |
| (parking on one side) | 60' | 31'*** | 0.3% |
| (no parking either side) | 60' | 26'*** | 0.3% |
| Cul-de-sac Street | 60' | 37' | 0.3% |
| (parking on one side) | 60' | 31' | 0.3% |
| (no parking either side) | 60' | 26' | 0.3% |
| Frontage Street | 40' | 29' | 0.3% |
| Alley | 16' | 16' | 0.3% |

| Commercial or Industrial Areas | | | |
|----------------------------------|--------|----------|-------|
| | R.O.W. | Improved | Grade |
| Arterial Street Five Lane Design | 100' | 65'*** | 0.3% |
| Arterial Street | 80' | 47'*** | 0.3% |
| Collector Street | 60' | 45'*** | 0.3% |
| Local Street | 60' | 41' | 0.3% |
| (parking on one side) | 60 | 41' | 0.3% |
| (no parking either side) | 60 | 41' | 0.3% |
| Cul-de-sac Street | 60' | 41' | 0.3% |
| (parking on one side) | 60' | 41' | 0.3% |
| (no parking either side) | 60' | 41' | 0.3% |
| Frontage Street | 65' | 41' | 0.3% |
| Alley | 24' | 22' | 0.3% |

Medians

Location: Medians may be located within streets to be dedicated to the public.

Lane width: A minimum unobstructed lane width of twenty (20.0) feet (improved 21' back of curb to back of curb) is required between the median curb and the street curb.

Landscaping: Medians within the public right-of-way may be landscaped, maintenance of landscaped medians shall be the responsibility of a property owners association created at or before filing the final plat. The city will maintain concrete medians or bricked medians within the public right-of-way.

All streets shall be designed and graded to the full right-of-way widths stated.

~~* The developer shall not be responsible for providing improvements wider than 37 feet at his expense.~~

~~** The developer shall not be responsible for providing improvements wider than 41 feet at developer's sole expense.~~

ORDINANCE NO. 9480 (Cont.)

~~*** A residential local street or cul de sac street improvement width of 31 feet (parking on one side); or, a residential local street or cul de sac street improvement width of 26 feet (no parking either side) shall normally not be permitted, and shall only be considered under special exceptional circumstances.~~

(9) Drives, streets, or roadways within condominium, townhouse, or planned unit developments shall have a minimum right-of-way, improvement, and grade as determined by agreement between the subdivider, public works director, director of Planning, and approved by the city council.

(10) The horizontal alignment on all streets except in unusual cases shall be as follows:

| | |
|--|--------------|
| Radii of Horizontal Curves (Center Line) | |
| Arterial Streets..... | 700' minimum |
| Collector Streets..... | 300' minimum |
| Local Streets..... | 100' minimum |

SECTION 2. Addendum "C" is hereby adopted as part of Chapter 33 of the Grand Island City Code, and shall read as follows

ADDENDUM "C"
APPROVED STREET CROSS SECTIONS

(As Attached)

SECTION 3. Sections 33-12 as now existing, and any ordinances or parts of ordinances in conflict herewith be, and hereby are, repealed.

SECTION 4. Sections 33-12 and Addendum "C" are hereby adopted as part of Chapter 33 of the Grand Island City Code.

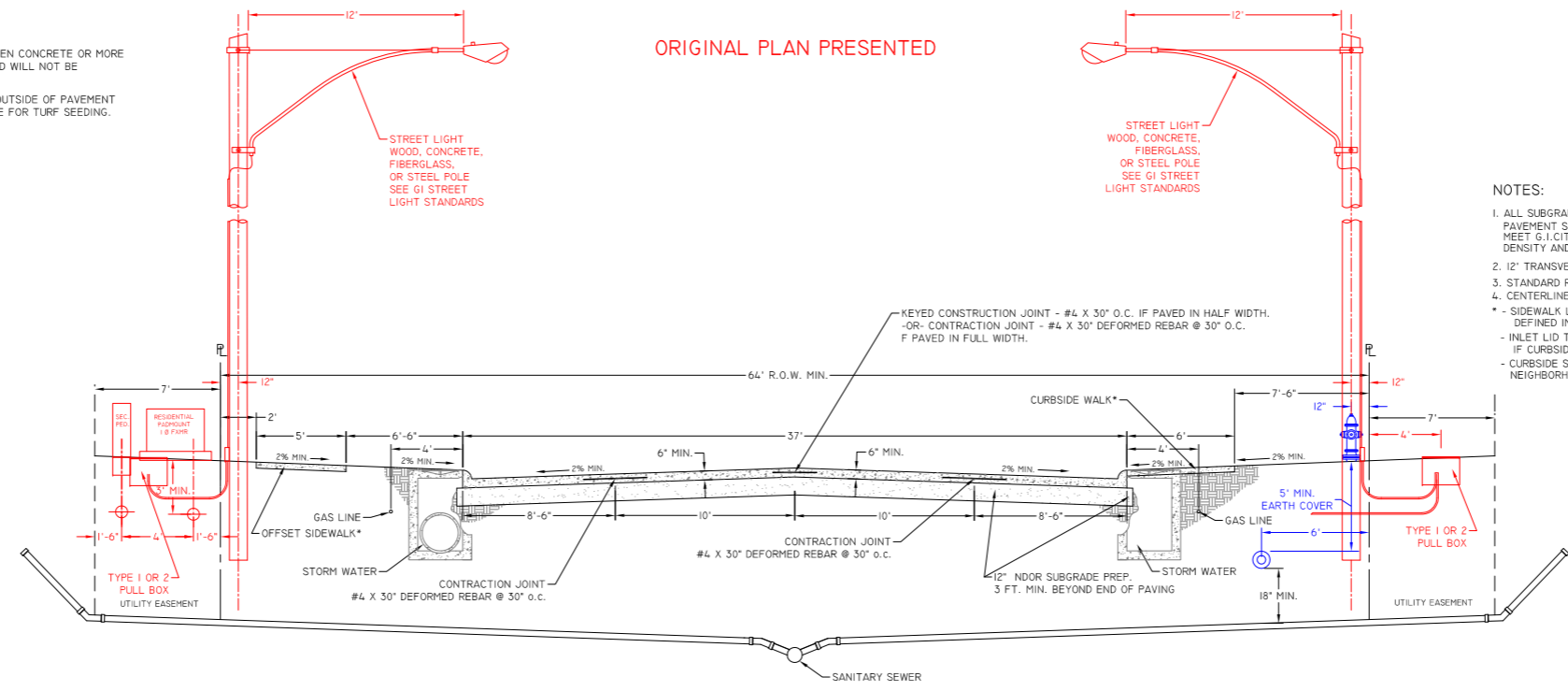
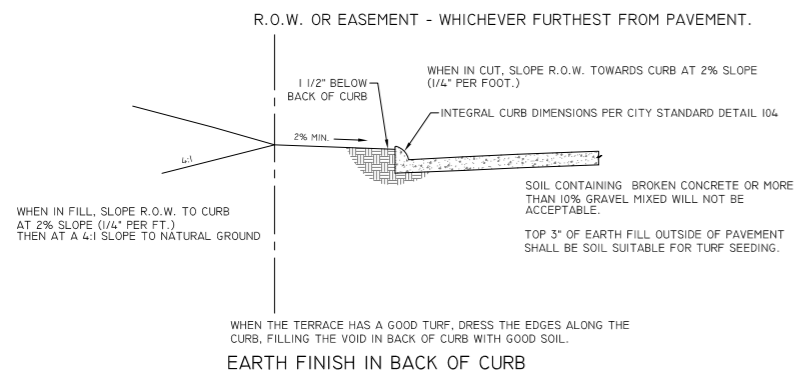
SECTION 5. That this ordinance shall be in force and take effect from and after its passage and publication, within fifteen days in one issue of the Grand Island Independent as provided by law.

Enacted: March 25, 2014.

Jay Vavricek, Mayor

Attest:

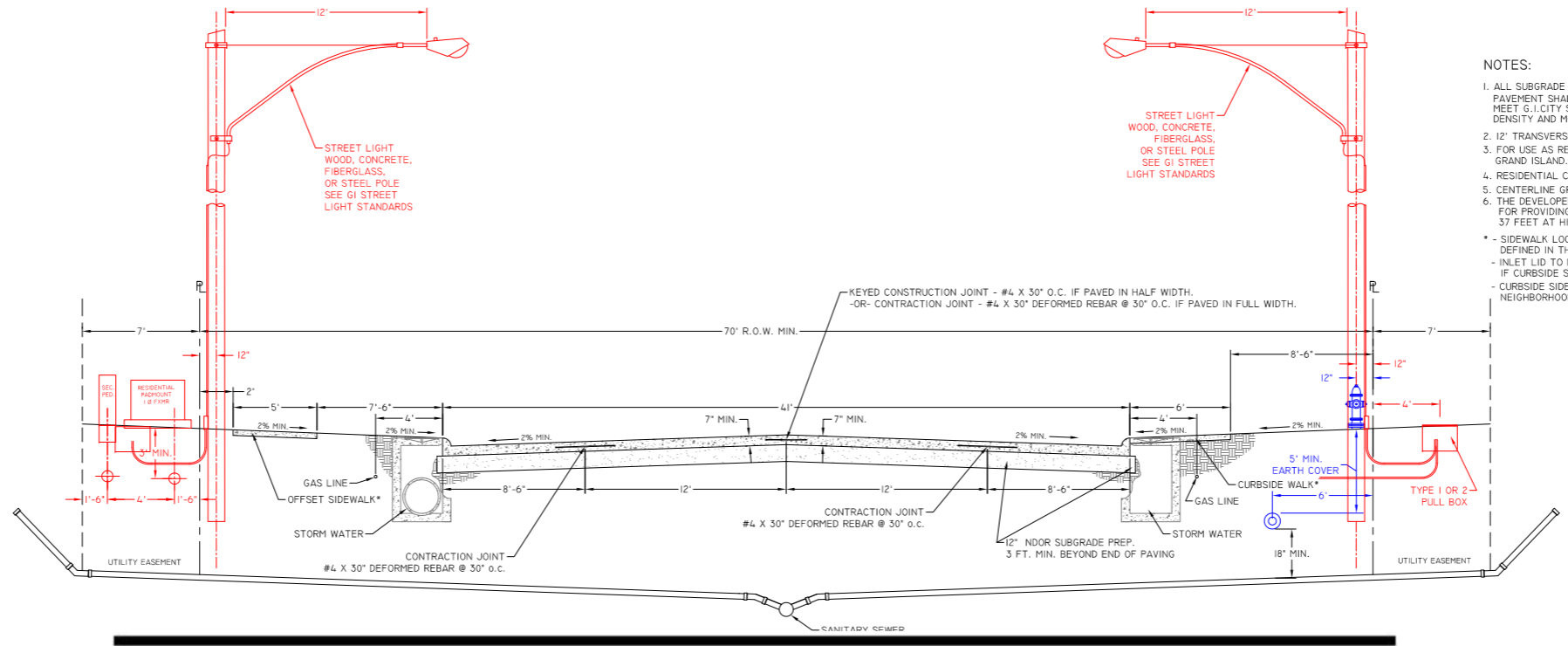
RaNae Edwards, City Clerk



ORIGINAL PLAN PRESENTED

STANDARD RESIDENTIAL USE
TYPICAL 37' PAVEMENT WIDTH

- NOTES:
1. ALL SUBGRADE AND FILL BELOW PAVEMENT SHALL BE TESTED AND MEET G.I. CITY STANDARD SPECS. FOR DENSITY AND MOISTURE.
 2. 12\"/>



RESIDENTIAL COLLECTOR
TYPICAL 70' PAVEMENT WIDTH

- NOTES:
1. ALL SUBGRADE AND FILL BELOW PAVEMENT SHALL BE TESTED AND MEET G.I. CITY STANDARD SPECS. FOR DENSITY AND MOISTURE.
 2. 12\"/>

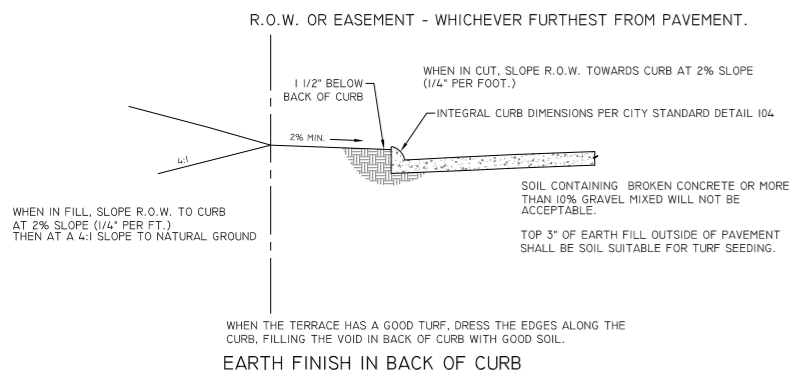
DWG: CAD DETAILS TYPICAL STREET SECTIONS CROSS SECTION RESIDENTIAL DWG - 37' & 41' II X 17 LANDSCAPE

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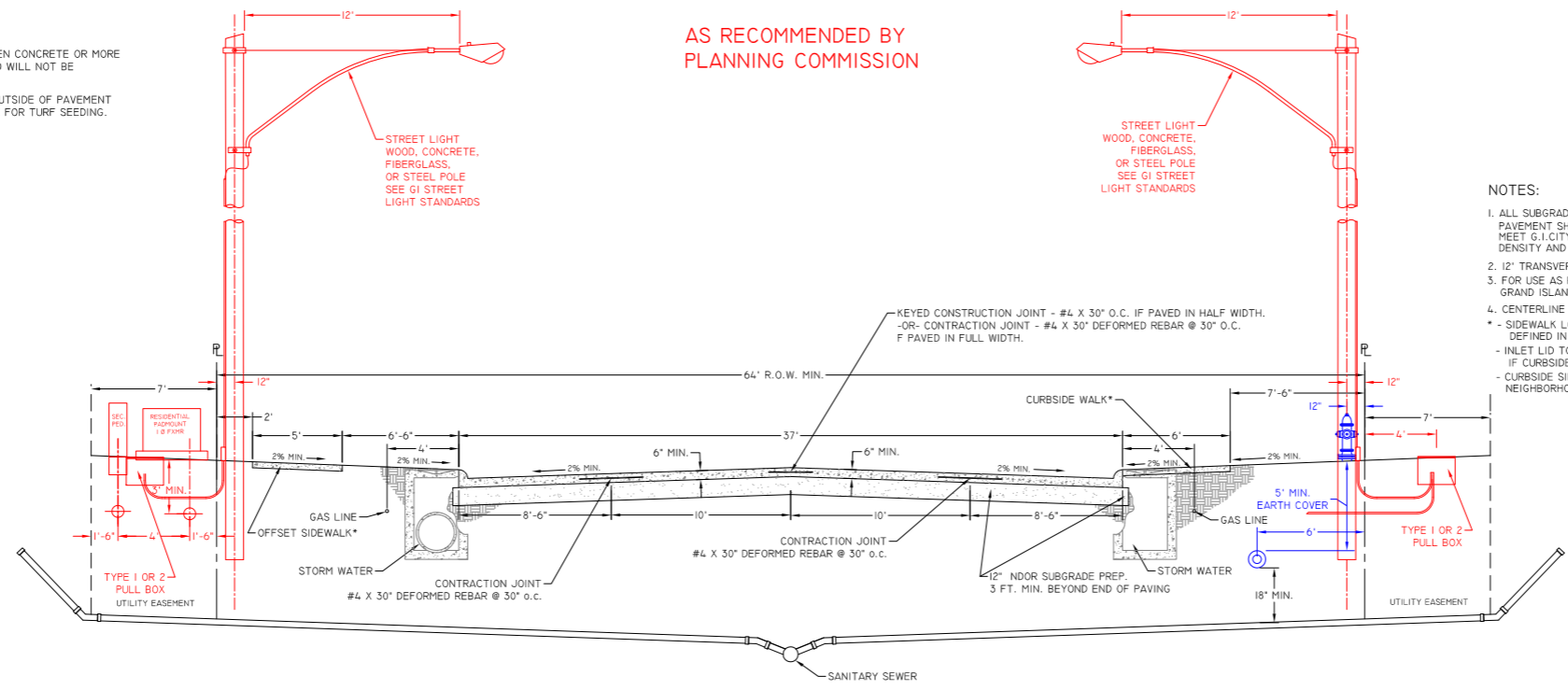
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| SURV. BY: | DATE: | 12/10/2013 |
| DR. BY: | DATE: | T-J |
| CHK. BY: | DATE: | XX/XX/XX |
| APP. BY: | DATE: | XX/XX/XX |
| DATE: | | XX/XX/XX |

RESIDENTIAL
PAYMENT CROSS SECTIONS
GRAND ISLAND, NEBRASKA

DWG: J:\CADD\DETAILS\TYPICAL STREET SECTIONS\CROSS SECTION-RESIDENTIAL - AS RECOMMENDED BY PLANNING COMMISSION 05-05-2014.DWG - 37' & 41' II X I7 LANDSCAPE

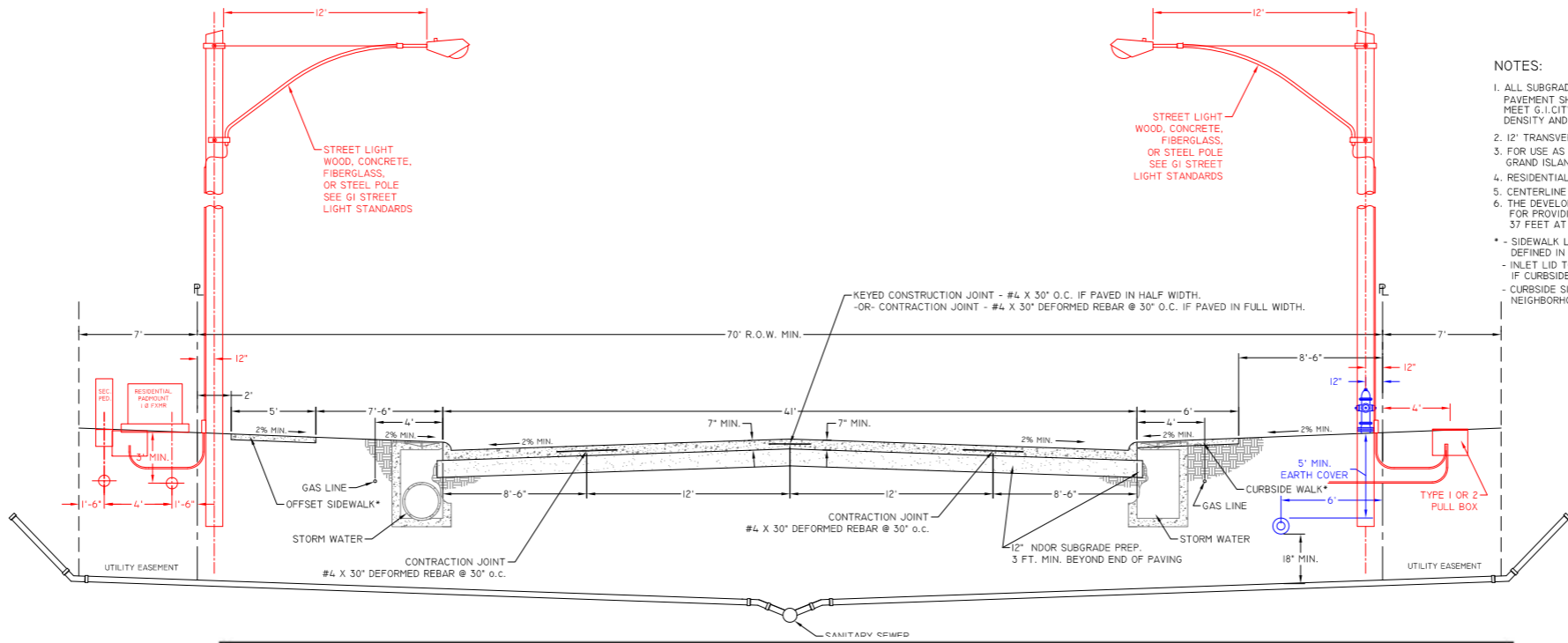


AS RECOMMENDED BY
PLANNING COMMISSION



- NOTES:**
1. ALL SUBGRADE AND FILL BELOW PAVEMENT SHALL BE TESTED AND MEET G.I. CITY STANDARD SPECS. FOR DENSITY AND MOISTURE.
 2. 12" TRANSVERSE JOINTS
 3. FOR USE AS REQUIRED BY THE CITY OF GRAND ISLAND.
 4. CENTERLINE GRADE MIN. 0.3%.
- * - SIDEWALK LOCATION TO BE LOCATED AS DEFINED IN THE SUBDIVISION AGREEMENT
 - INLET LID TO MATCH SIDEWALK CROSS SLOPE IF CURBSIDE SIDEWALK INSTALLED
 - CURBSIDE SIDEWALK ONLY PERMITTED WHERE NEIGHBORHOOD MAILBOXES ARE PROVIDED.

WIDE RESIDENTIAL USE
TYPICAL 37' PAVEMENT WIDTH



- NOTES:**
1. ALL SUBGRADE AND FILL BELOW PAVEMENT SHALL BE TESTED AND MEET G.I. CITY STANDARD SPECS. FOR DENSITY AND MOISTURE.
 2. 12" TRANSVERSE JOINTS
 3. FOR USE AS REQUIRED BY THE CITY OF GRAND ISLAND.
 4. RESIDENTIAL COLLECTOR.
 5. CENTERLINE GRADE MIN. 0.3%.
 6. THE DEVELOPER SHALL NOT BE RESPONSIBLE FOR PROVIDING IMPROVEMENTS WIDER THAN 37 FEET AT HIS EXPENSE.
- * - SIDEWALK LOCATION TO BE LOCATED AS DEFINED IN THE SUBDIVISION AGREEMENT
 - INLET LID TO MATCH SIDEWALK CROSS SLOPE IF CURBSIDE SIDEWALK INSTALLED
 - CURBSIDE SIDEWALK ONLY PERMITTED WHERE NEIGHBORHOOD MAILBOXES ARE PROVIDED.

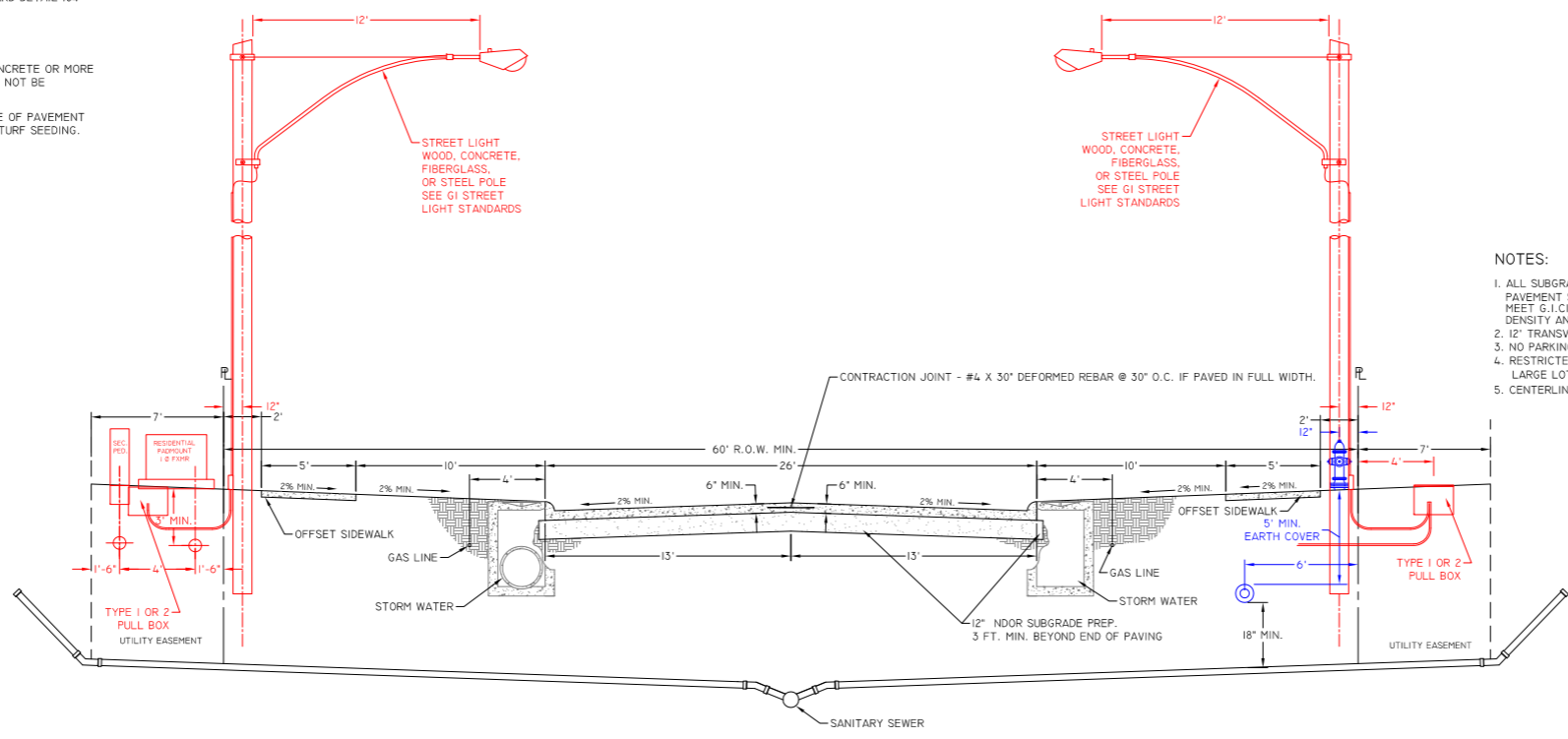
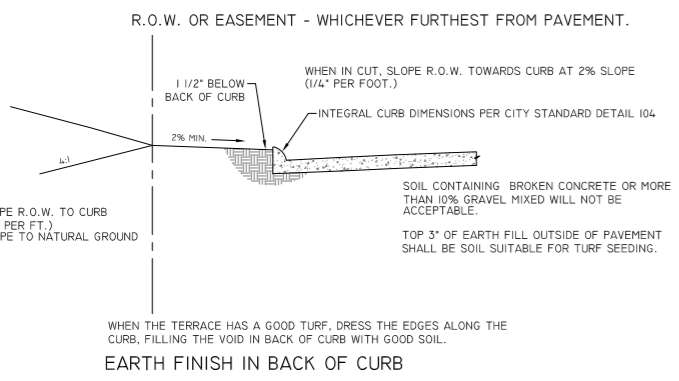
RESIDENTIAL COLLECTOR
Council Session (Corrected) - 3/25/2014

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| RESIDENTIAL | PAVEMENT CROSS SECTIONS GRAND ISLAND, NEBRASKA |
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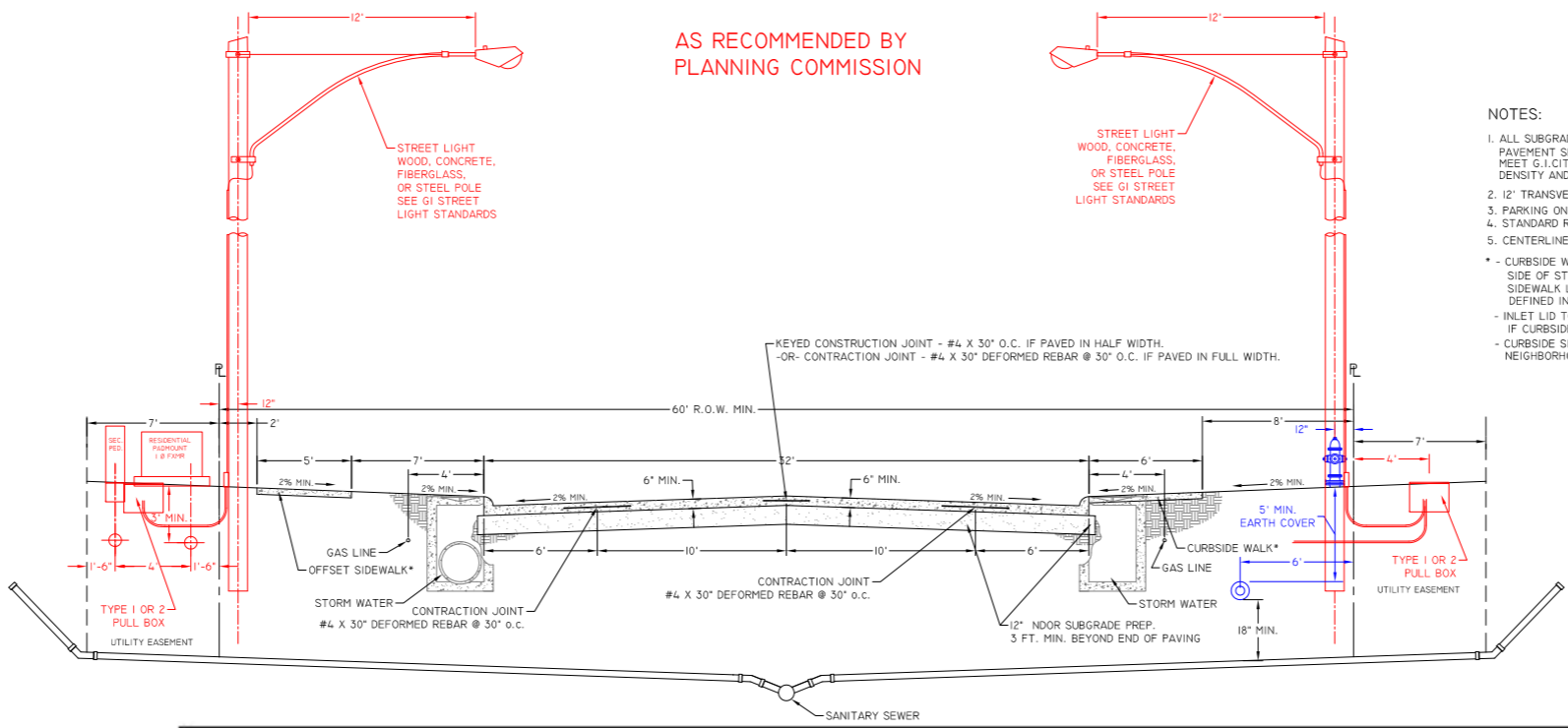
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DWG: H:\CAD\DETAILS\TYPICAL STREET SECTIONS\CROSS SECTION-RESIDENTIAL - AS RECOMMENDED BY PLANNING COMMISSION 03-05-2014.DWG - 26' & 32' II X 17 LANDSCAPE



LARGE LOT RESIDENTIAL DEVELOPMENT
TYPICAL 26' PAVEMENT WIDTH

- NOTES:**
1. ALL SUBGRADE AND FILL BELOW PAVEMENT SHALL BE TESTED AND MEET G.I. CITY STANDARD SPECS. FOR DENSITY AND MOISTURE.
 2. 12" TRANSVERSE JOINTS
 3. NO PARKING
 4. RESTRICTED TO USE IN LLR - LARGE LOT RESIDENTIAL DEVELOPMENT.
 5. CENTERLINE GRADE MIN. 0.3%.



STANDARD RESIDENTIAL STREET
Council Session (Corrected) - 3/25/2014

- NOTES:**
1. ALL SUBGRADE AND FILL BELOW PAVEMENT SHALL BE TESTED AND MEET G.I. CITY STANDARD SPECS. FOR DENSITY AND MOISTURE.
 2. 12" TRANSVERSE JOINTS
 3. PARKING ONE SIDE ONLY.
 4. STANDARD RESIDENTIAL USE.
 5. CENTERLINE GRADE MIN. 0.3%.
- CURBSIDE WALK LOCATED ON PARKING SIDE OF STREET.
 - SIDEWALK LOCATION TO BE LOCATED AS DEFINED IN THE SUBDIVISION AGREEMENT
 - INLET LID TO MATCH SIDEWALK CROSS SLOPE IF CURBSIDE SIDEWALK INSTALLED
 - CURBSIDE SIDEWALK ONLY PERMITTED WHERE NEIGHBORHOOD MAILBOXES ARE PROVIDED.

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| SURV. BY: _____ DATE: 12/10/2013 DR. BY: T.JY DATE: XX/XX/XX CHK. BY: _____ DATE: XX/XX/XX APPR. BY: _____ DATE: XX/XX/XX | RESIDENTIAL PAVEMENT CROSS SECTIONS GRAND ISLAND, NEBRASKA |
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